



James Fleming | Architectural



Mr D Collins - Proposed Full Application for a Residential Development, Land off London Road, Hinckley

Design & Access Statement

Date - 26.06.25

JFA2024\_12 Full App Rev -

## Contents

---

### **1.0 - Introduction**

- 1.1 - Overview
- 1.2 - Design Team
- 1.3 - Site Location

### **2.0 - Project Background**

- 2.1 - The Site

### **3.0 - Site Analysis and Context**

- 3.1 - Development Context
- 3.2 - Contextual Analysis
- 3.3 - Site Constraints
- 3.4 - Flood Assessment
- 3.5 - SWOT Analysis
- 3.6 - Drainage, Services, Easements and Noise Impact
- 3.7 - Ecological Consideration

### **4.0 - Design**

- 4.1 - Key Design Principles

### **5.0 - Development Proposals**

- 5.1 - Proposed Site Layout
- 5.2 - Proposed Building Design
- 5.3 - Indicative Street Elevation
- 5.4 - Amount
- 5.5 - External Material Use

### **6.0 - Access**

- 6.1 - Site Access

### **7.0 - Summary**

- 7.1 - Conclusion

## 1.1 Overview

This document has been produced by James Fleming Architectural Ltd on behalf of the client to support Full Planning application to develop the site, on the land off London Road, Hinckley (Adjacent Tesco Express).

The proposal seeks approval for a residential development of 3 dwellings on land currently vacant, but with a last known lawful use of Residential.

The following Design & Access Statement is to be read in conjunction with wider planning application drawings, documents and details accompanying this application.

This document illustrates how key design opportunities have been maximised, how constraints have been overcome and how good design principles have shaped the proposed development into a responsive, vibrant and contextually appropriate proposal.



## 1.2 Design Team

---

**Design Team;**

James Fleming Architectural Ltd

50 Leicester Road

Hinckley

Leicestershire

LE10 1LT



James Fleming | Architectural

**Client:**

Mr D Collins

50 Woodbank,

Burbage,





LE10 2BY

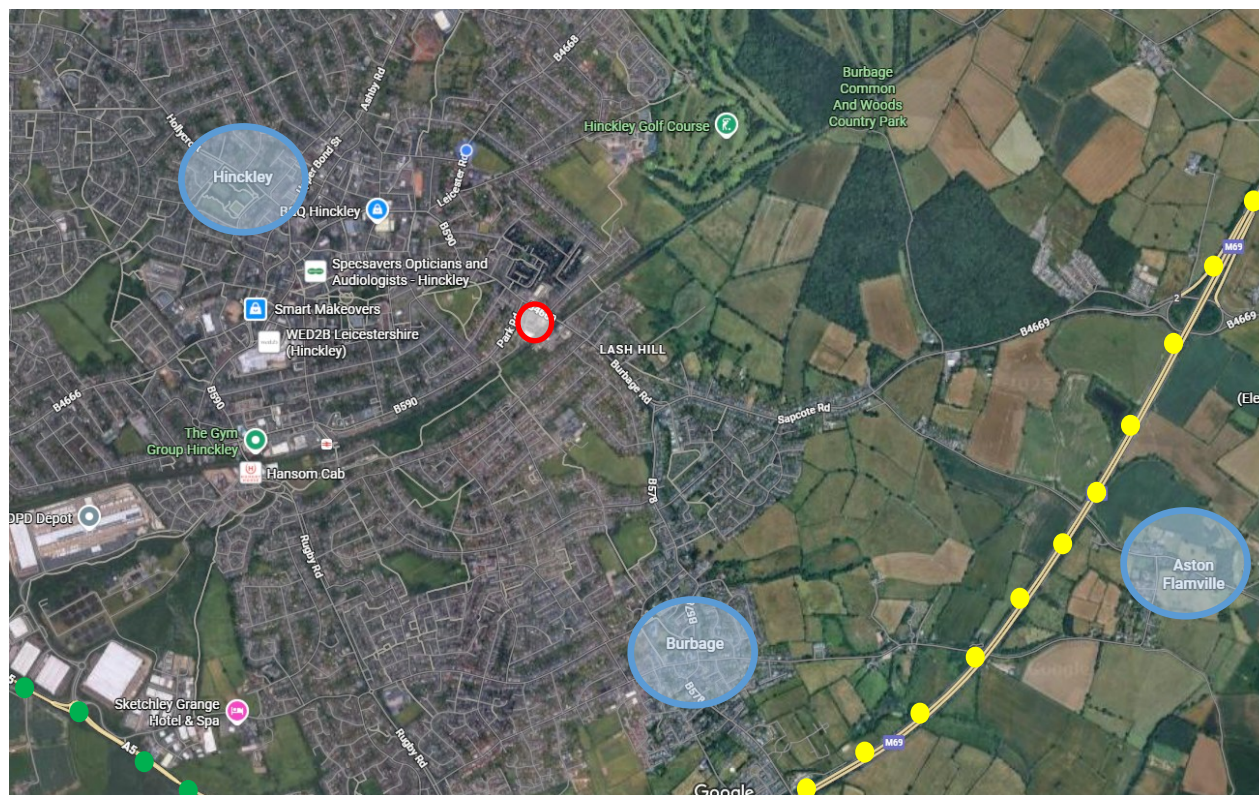
### 1.3 Site Location

The proposed site (extract from Google Maps) within Hinckley is set just on the border to Burbage. The site is within a 5 minute walk of Burbage, a short drive to the local motorway network and of course adjacent to Hinckley Town centre.

Numerous bus services link the above mentioned towns and villages, each leading to and offering multiple shopping retail parks, independent shops, schools, Hospital, play and open space.

The site is set in a majority residential weighted area with pockets of independent retail outlets. Established Infrastructure and pedestrian links to local amenities, and

-  Key Motorway links
-  Key A road links
-  Site
-  Commutable towns & villages





## 2.1 The Site

### The Site;

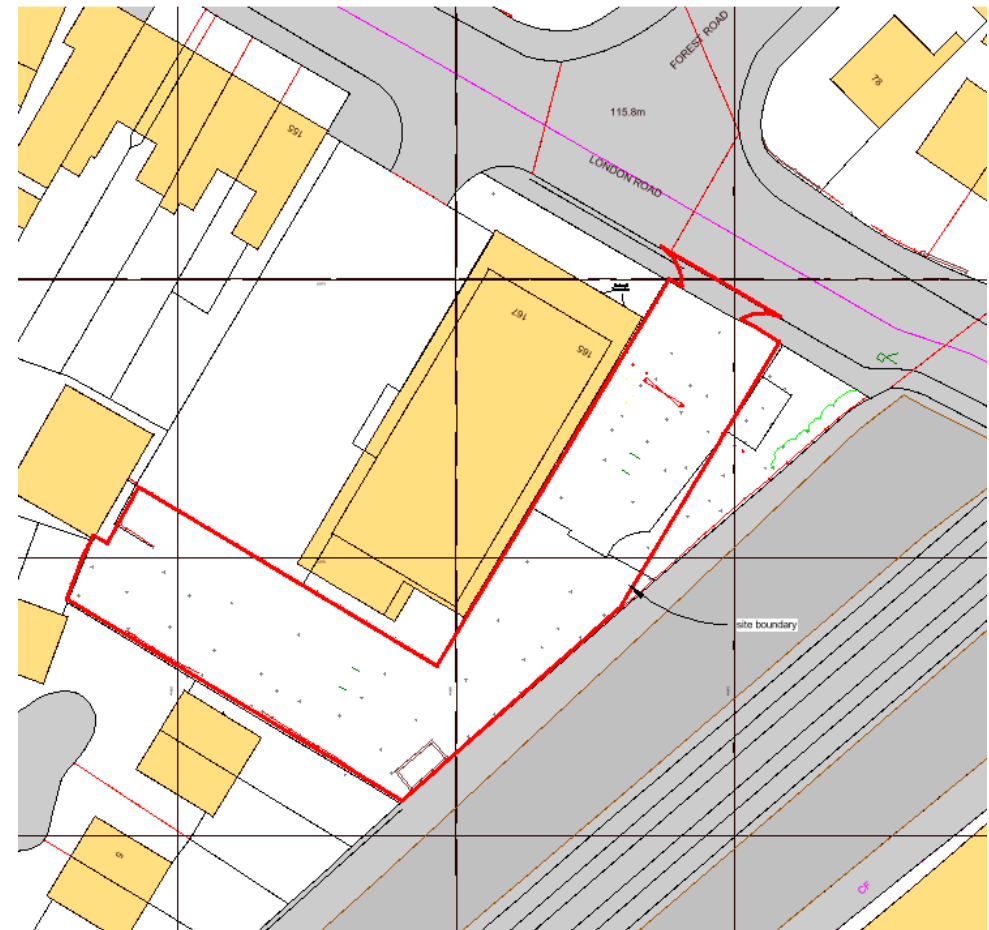
The 812sq.m site is located to the southern side of London Road, set adjacent to the railway line to the East and Tesco Express to the West. Residential occupies land to the north, and south.

The site boundaries contain a mix of boundary fencing bunding the former residential use on the land, some replaced in more recent times as adjacent development has occurred.

The current site comprises a hard standing finish with a single point of controlled access off London Road.

The site is reasonably level with a gentle fall to the southern rear boundary.

The site was recently subject to a successful Outline Planning Application, seeking approval for a new vehicular access to serve up to 3 residential units. The Outline application ref was 24/01156/OUT.



### 3.1 - Development Context

As part of the design development process, a contextual study has been undertaken to gain an insight into the pattern of adjacent development and range of materials and architectural detailing that lie within close proximity to the site. The following pages of site analysis identify key considerations which have influenced the design process and associated decision making.

The map in 'figure 1', demonstrates a bias towards a linear form of development in the local vicinity, with residentially units typically fronting the public highway, on primary and secondary roads with smaller cul-de-sac tertiary roads on more recent developments.



Site

Figure 1— Local Map demonstrating typical form of development

### 3.1.1 - Development Context



Image 1— Site Entrance and frontage to Tesco Express next door



Image 2—Tesco Express car park entrance with early 20th Century period houses



Image 3—Early 20th Century Period terrace homes that have been modernised and altered over time



Image 4—Mid 20th Century Homes opposite Tesco Express, comprising differing built form and design



Image 5—Southern view along London Road outside of site access, highlighting road narrowing



Image 6—Northern view a long London Road, picking up road junction to Forest Road



## 3.1.2 - Development Context



-  Site Access
-  Photo location

Site Plan to locate the contextual analysis images.



## 3.2 - Contextual Analysis



Image 1—Ariel view of existing site. Highlights a general linear built form surrounding the site boundaries.

### 3.3 - Site Constraints



1. Existing 'T' junction off London Road into Forest Road.
2. Access into the neighbouring Tesco Express.
3. Reduction in pedestrian space on the southern side of London Road, heading Easterly from site.
4. Train line to the East.
5. Neighbouring 'Tesco Express' with modern form of design, out of context with street scene.
6. Existing residential dwellings, set on tertiary road, establishing a build line.
7. Early 20th century period dwellings with distinctive design, including stone and brick detailing, flat roof bay roof projections to the front, mix of fenestration styles.
8. Mid 20th century dwellings with a variety of built form and elevation design.



### 3.4 - Flood Assessment

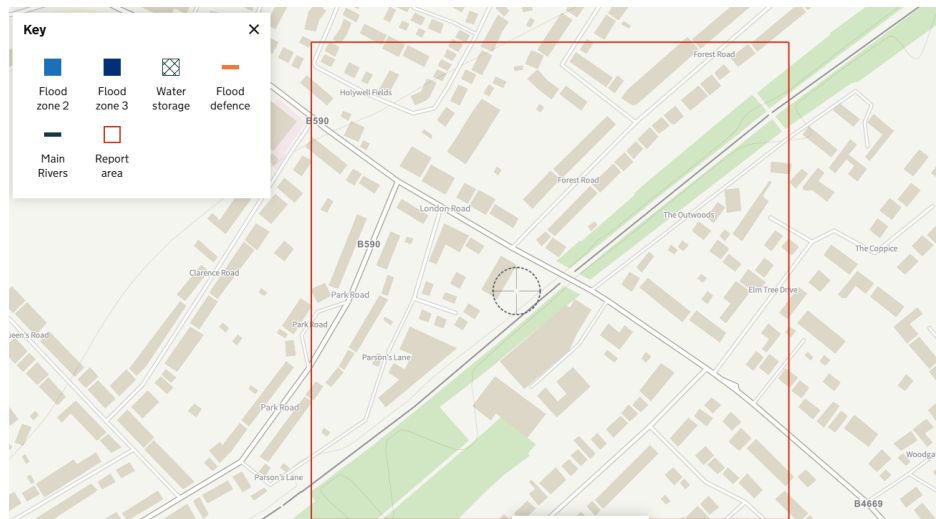


Image 1—Environment Agency mapping confirms location of site is outside of mapped flood risk zones from rivers or the sea.

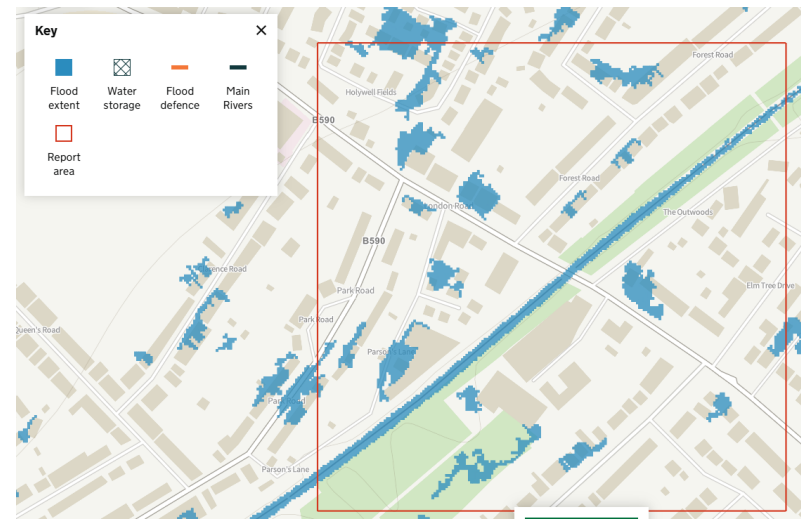


Image 2—Environment Agency mapping confirms location of site is outside of mapped flood risk zones from surface water. The adjacent area is set at the base of the adjacent railway line, some 5-10m lower than our site.



### 3.5 - SWOT Analysis

---

#### Strengths:

- The site is part of a well-established mixed use residential / commercial area.
- Good services within site boundary
- Good accesses to bus routes.
- Access to nearby leisure walking routes including an established public footpath network.
- Topography is gentle and consistent away from the highway
- Sustainable relationship to existing Schools, Doctors and retail units.
- Benefit of recent Outline Planning Approval to support a new vehicular access of London Road for the density of development proposed.
- Last lawful planning use of the site is Residential.

#### Weaknesses:

- Next to a train line.
- No existing access vehicular into the site.
- Poor Biodiversity value as the site was stoned over as part of the Tesco Express construction process.

#### Opportunities:

- Use the assets around the perimeter of the site to create a strong setting for residential development with a public realm focus.
- To improve the visual impact of an otherwise vacant plot of land, left unmanaged becomes an eyesore.
- Potential for improvement to the boundary landscape buffers
- To infill an open void to create a linear link between existing buildings on London Road.
- To increase the Biodiversity value of the site.

#### Threats:

- Short term disruption whilst the building works are on going.
- Current long range clear views from neighbouring windows will be reduced, but the view of unkept land will be removed and therefore improved.

### 3.6 - Drainage, Services, Easements and Noise Impact.

---

#### Drainage

The nature of the development will require the installation of new below ground surface and foul water drainage systems with new connections to be formed within the existing public foul and storm sewers located either within or adjacent to our site boundary.

A SUDS system will also be designed and installed to allow for permeable discharge of storm water from private hardstanding areas.

#### Easements

No Easements affect the site to our knowledge.

#### Services

The site is currently served by all utilities with infrastructure located on surrounding roads.

#### Noise Impact

The site is adjacent to the local railway line and we anticipate the appropriate method to mitigate noise impact on the new dwellings are as follows:

1. Acoustic vents to habitable windows facing the railway line.

2. No habitable windows on railway facing elevations where the dwelling is closest to the southern boundary.
3. Soft landscaped buffer with fence, and car parking, all set between the boundary and frontage of set back dwellings.
4. Where necessary fixed window lights can be offered if suitable mechanical ventilation is offered to compensate.

We do not envisage the noise impact to be adverse, particularly as the dwellings to the south of our Plot 3, have been delivered in the last 20 years, with each comprising habitable windows facing the railway line, at a similar distance away to the proposed scheme.

We would request a planning condition be applied if a Noise Impact Assessment is deemed necessary to advise of the recommended mitigation methods to adopt.

### 3.7 - Ecological Consideration

#### Landscape

It is likely the site will need to demonstrate some form of Net Ecological gain, a number of ways this may be achieved and we propose the consideration of the following are appropriate for this site:

- Bat & Bird Boxes fixed to trees / buildings.
- Planting of native deciduous tree species.
- Wildflower seeding areas, use of flowering lawn seed mixes.
- Introduction of bat and bird friendly native planting and wildflower hedge-row edge mix seeding.

The proposed site plan highlights where these areas shall be located.

The current site has no soft landscaping due to being used as a builders yard while Tesco Express was constructed.

The proposals will naturally introduce areas of soft landscaping via a detailed scheme with implementation strategy, that we request to be provided subject to a planning condition.

#### Tree Survey

- There are currently no trees on the site or adjacent that may impact the development.

#### Boundary Treatment

All boundary treatments to the perimeter of the site are to be retained where they abut neighbouring dwellings and Tesco Express.

New internally set plot boundaries will comprise 1.8m high close boarded timber fences, with timber posts and timber gravel boards.

New boundary treatment to private amenity spaces, facing the public realm will comprise a minimum 2m high facing brick (to match dwelling) walls, with brick on edge and tile creasing coping.

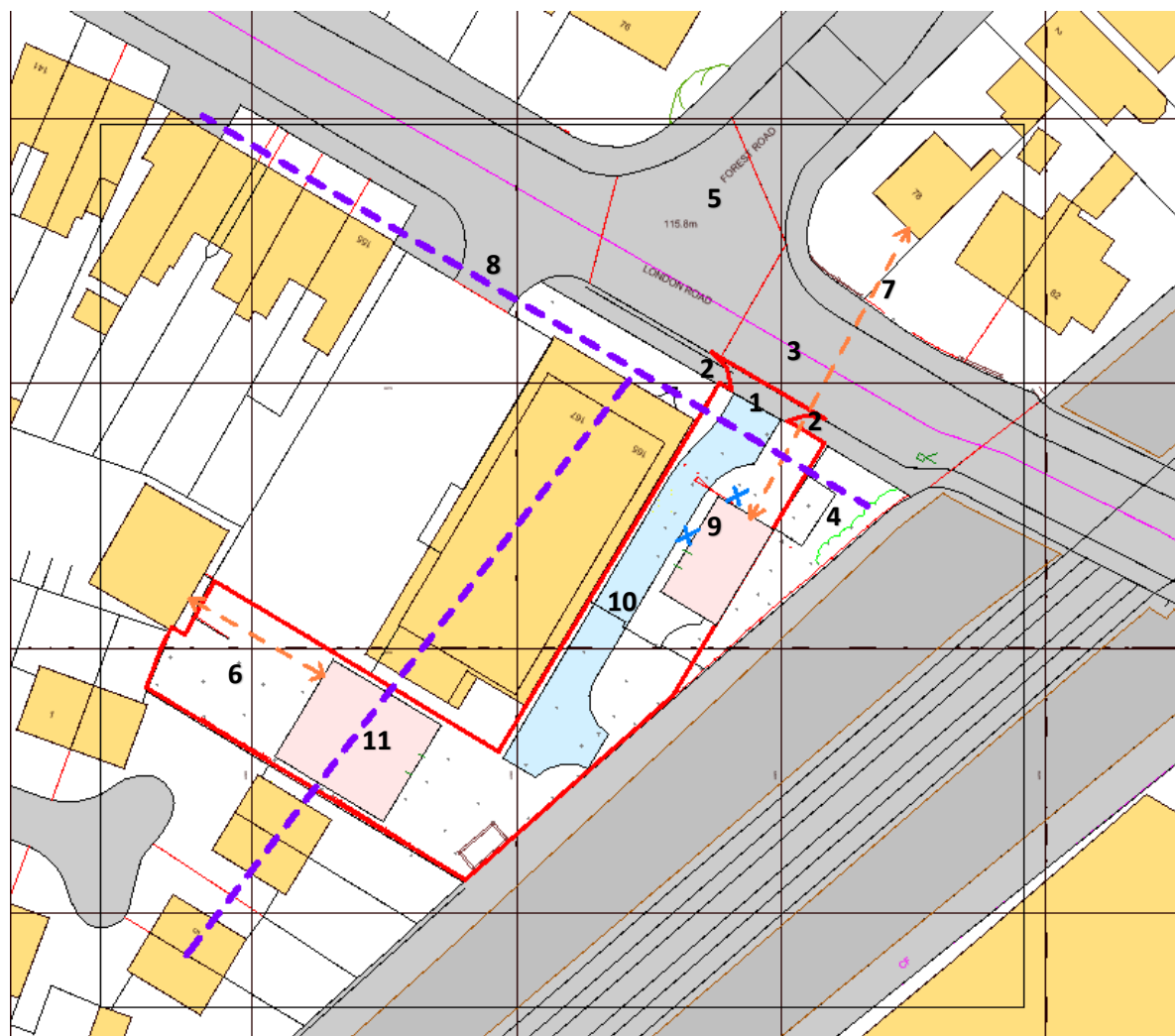
The public realm fronting the railway line shall be treated with soft landscaping and a post and rail stock fence (to guard) to retain a sense of openness to the site.

#### Ecology

The site is completely hard standing and any alteration would impact on less than 25 m<sup>2</sup> of habitat/5m of linear habitat, therefore, we understand the biodiversity net gain policy does not exist to this site, defined to fall within the 'de minimis exemption' category.

The proposals will naturally introduce areas of soft landscaping and Ecological gains as referred under 'Landscape', so would request the detail of such be subject to planning condition.

## 4.1 - Key Design Principles



### Key Considerations;

1. Introduce a new vehicular access off London Road.
2. Consider both vehicular and pedestrian visibility plays.
3. Consider existing traffic speed and quantum along London Road.
4. Relationship to the bridge.
5. Consider local access roads.
6. Consider distance to existing commercial building.
7. Consider distance between habitable windows.
8. Consider new home frontage line, to mimic existing local dwellings offering a small public realm garden.
9. Opportunity for dual fronted dwelling on corner plot.
10. New access drive off London Road.
11. New housing block maintaining linear grain.



## 5.1 - Proposed Site Layout

**Key Considerations;**

1. Access located to offer adequate visibility splays.
2. Layout confirms future buildings can be located to comply with Approved Document B of the Building Regulations, by means of fire brigade access in the event of an emergency.
3. Proposed plan demonstrates bin storage and collection areas are within permitted allowances.
4. New access road has been assessed within the supporting Traffic Statement, following a traffic count survey, and we meet the local constraints and policies, and approved under recent Outline Approval.
5. Site vehicular turning.
6. 150% parking offered.
7. Private amenity to all plots, served with cycle storage and bin storage.

## 5.2 - Proposed Building Design



### House type T1;

#### Materials to comprise:

1. Concrete roof Tiles
2. uPVC Fascia's and soffits
3. Black uPVC rainwater goods.
4. Facing brickwork to wall.
5. Re-constituted stone cills, headers and bands.
6. GRP canopy over front door.
7. uPVC white windows and doors.

## 5.2.1 - Proposed Building Design



### House type T2;

#### Materials to comprise:

1. Concrete roof Tiles
2. uPVC Fascia's and soffits
3. Black uPVC rainwater goods.
4. Facing brickwork to wall.
5. Re-constituted stone cills, headers and bands.
6. GRP canopy over front door.
7. uPVC white windows and doors.

## 5.2.2 - Proposed Building Design



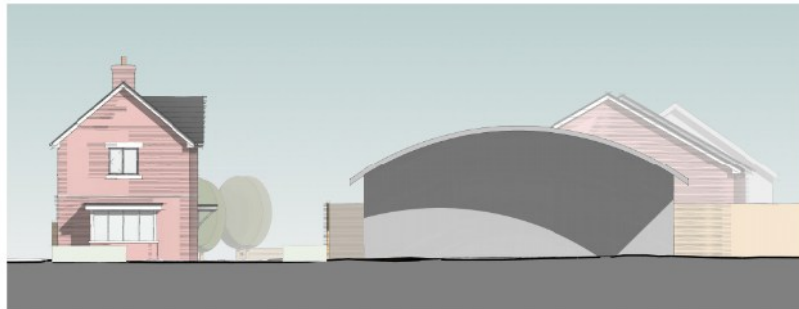
### House type T3;

#### Materials to comprise:

- Concrete roof Tiles
- uPVC Fascia's and soffits
- Black uPVC rainwater goods.
- Facing brickwork to wall.
- Re-constituted stone cills, headers and bands.
- GRP canopy over front door.
- uPVC white windows and doors.



## 5.3 - Indicative Street Elevation



Street Elevation - London Road  
1 : 100



Street Elevation - Westerly  
1 : 100



Street Elevation - Easterly  
1 : 100

Please note: should any discrepancies be noted, JAF shall be notified prior to the element of work being undertaken, to ensure any matters arising can be resolved without adverse works occurring.

All dimensions offered are indicative only and subject to change and prior to ordering any materials site checks shall be taken.

Soft & Hard Landscaping along with all external levels and 3D visuals are shown indicatively only.

0m 2m 4m 6m 8m 10m

SCALE 1:200 @ A2

Project No.	Shedding No.
2024 12	125 Ren
Drawing Name	
Street Elevations	
Project Details	
Mc Collins, Proposed Development, Land Adjacent Tesco Express, London Road, Runcorn	
Scale @ A2	
1 : 100	
Author	
JAF	
<small>CIAT is a registered trademark of the Construction Industry Training Board (CITB) and is used under license by CIAT. CIAT is a not-for-profit organisation and is not responsible for any errors or omissions in this document. CIAT is a registered charity (1134461) and is a member of the CITB group of companies.</small>	

### Street Elevation;

Typical street elevations from public realm to help illustrate the building form and relationship to each other.

## 5.4 - Amount

<b>Accommodation Schedule</b>				
Name	Type	No. Market Homes	Area (approx GIA sq.ft)	Total Area (approx GIA sq.ft)
T1	2 bed	1	850	850
T2	3 bed	1	1001	1001
T3	3 bed	1	1001	1001
	<b>TOTAL</b>	<b>3</b>		<b>2852</b>

**Amount;**

The site comprises a variety of 2 & 3 bedroom dwellings including. This mix caters for a strong market demand of both family homes and those first time buyers.

**Parking;**

150% car parking has been offered across the site with 1 visitor bay allocated.

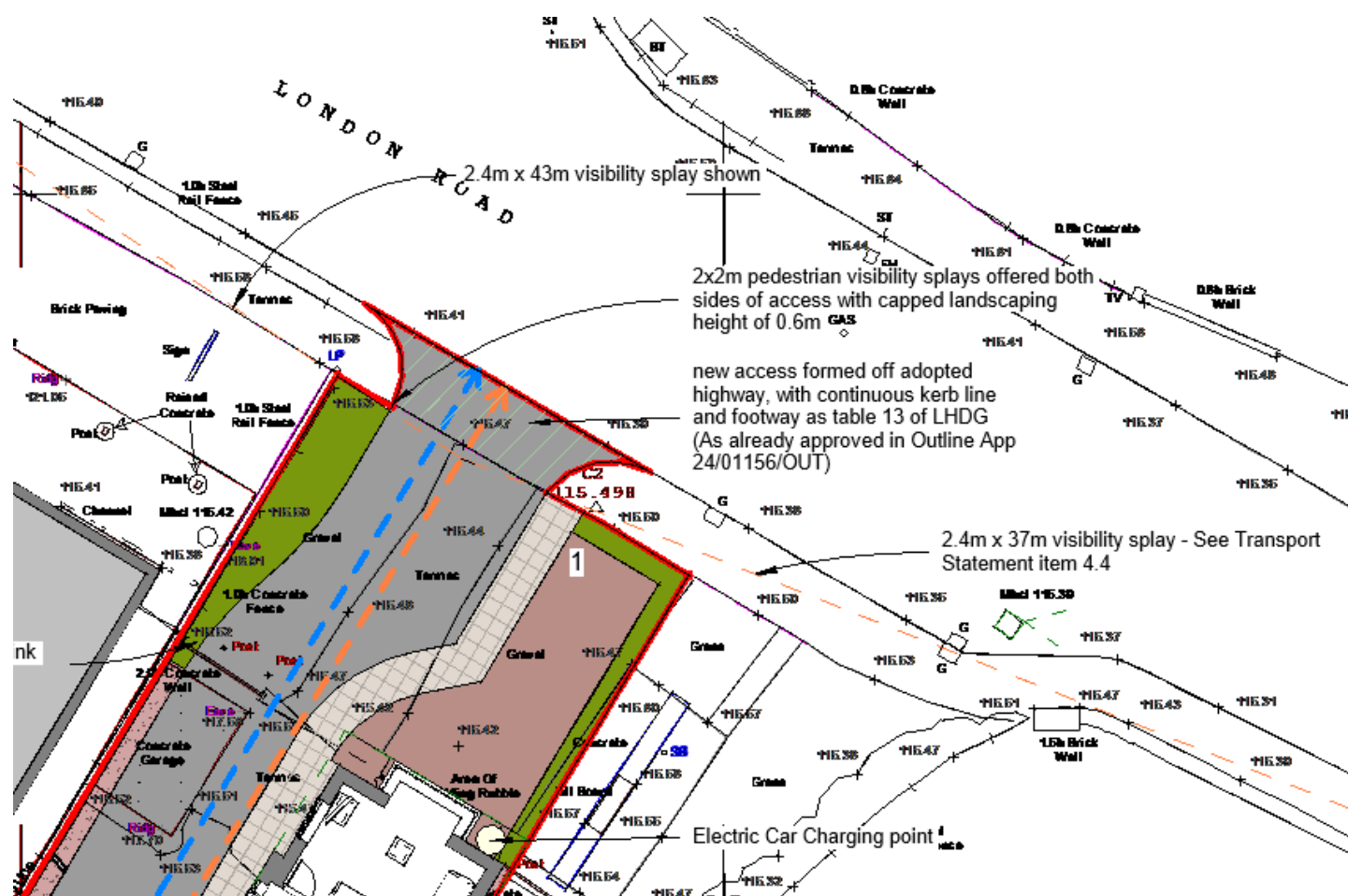
**Affordable Threshold;**

A total of 3 plots have been offered and therefore does not trigger the need for affordable homes to be catered for.

## 5.5 - External Material Use



## 6.1 - Site Access





## 7.1 - Conclusion

---

The proposed scheme will make efficient use of the site, creating an attractive residential environment which reinforces the existing residential context. The choice of a sympathetic residential style and the use of traditional, high quality external facing materials combined with landscaping to the site will ensure that the scheme significantly compliments its setting.

The scheme will aspire to make a positive contribution to the character and quality of the area through the creation of a safe and attractive residential development. The scheme will accord with the overall objectives of national and local policy in terms of urban design, promoting sustainable development, achieving good design and responding to the site conditions and local character.