



LAND WEST OF SHILTON ROAD, EARL SHILTON DESIGN AND ACCESS STATEMENT

NOVEMBER 2025

PROJECT DETAILS

Client: Giles Stanley Ltd
Project Name: Land West of Shilton Road, Earl Shilton
Reference Number: 25.034
Date: November 2025

This document was prepared by:

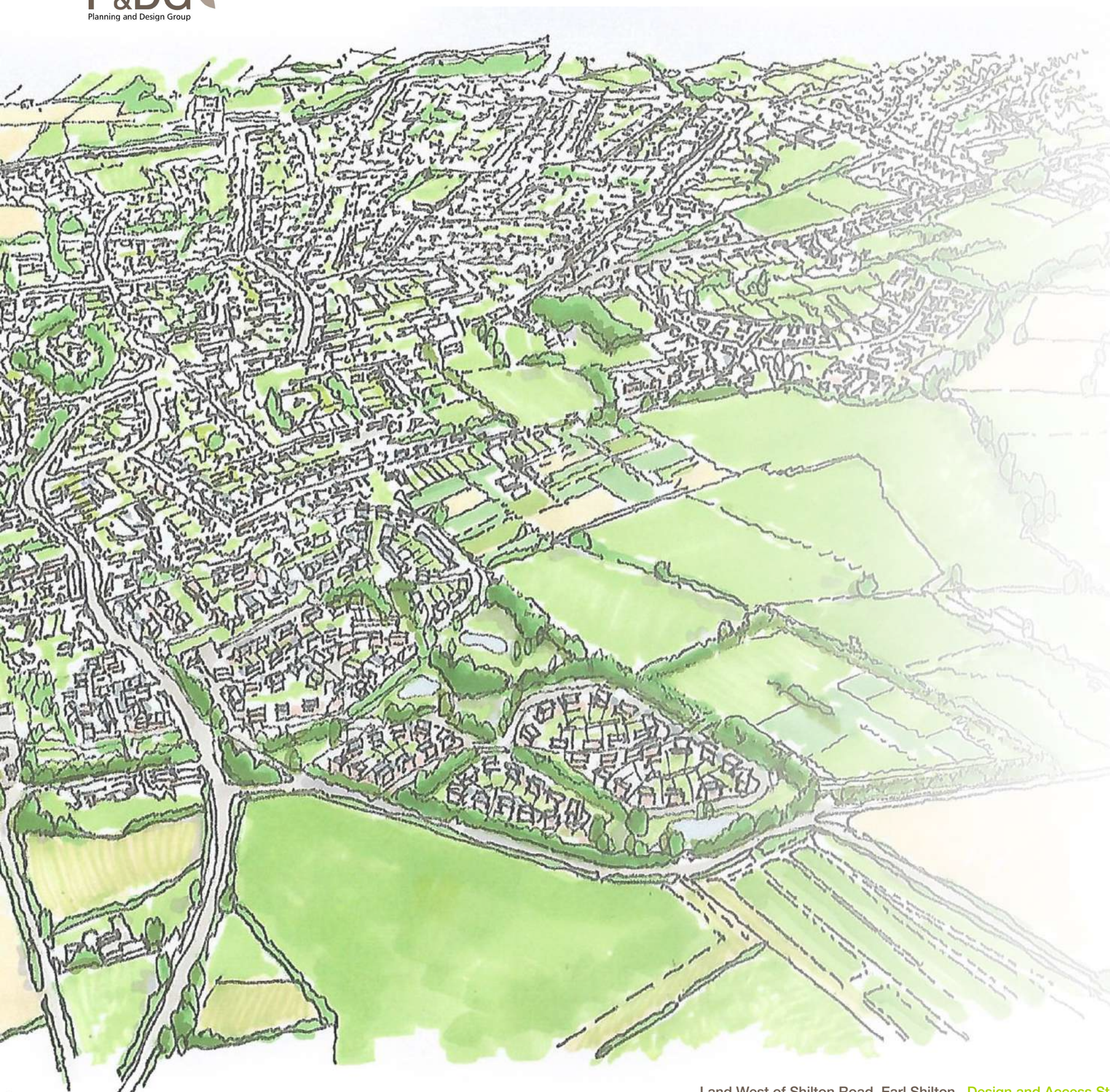


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On behalf of:

Giles Stanley Ltd



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LAND WEST OF SHILTON ROAD, EARL SHILTON

INTRODUCTION

Introduction

This supporting Design and Access Statement (DAS) has been prepared by Planning and Design Group (UK) Limited ('P&DG') on behalf of Giles Stanley Limited ('the applicant') in support of an outline planning application for residential development on land West of Shilton Road, Earl Shilton, Leicestershire ('the site'). The development will provide up to 120 dwellings including 20% of the total as affordable housing along with enhancements to public right of ways, creation of public amenity space and habitat creation.

The proposal has been prepared to help to address the recognised shortfall in meeting the Borough's housing needs (including affordable housing), as demonstrated by the current deficient supply of housing land, and so would support the Government's drive to significantly boost the supply of new homes. The site has been selected as it is within a broader area that the Council has previously recognised as suitable for residential development.

The development would deliver significant benefits across each of the three overarching objectives (economic, social and environmental) that contribute towards sustainable development.

This DAS, alongside other documents submitted in support of the application, demonstrates that the site is entirely suitable for residential development and that the overall planning balance weighs firmly in favour of supporting the proposal. Consequently, there are no reasons why the application should not be approved.



Figure 1: Site Aerial Location
(Google)

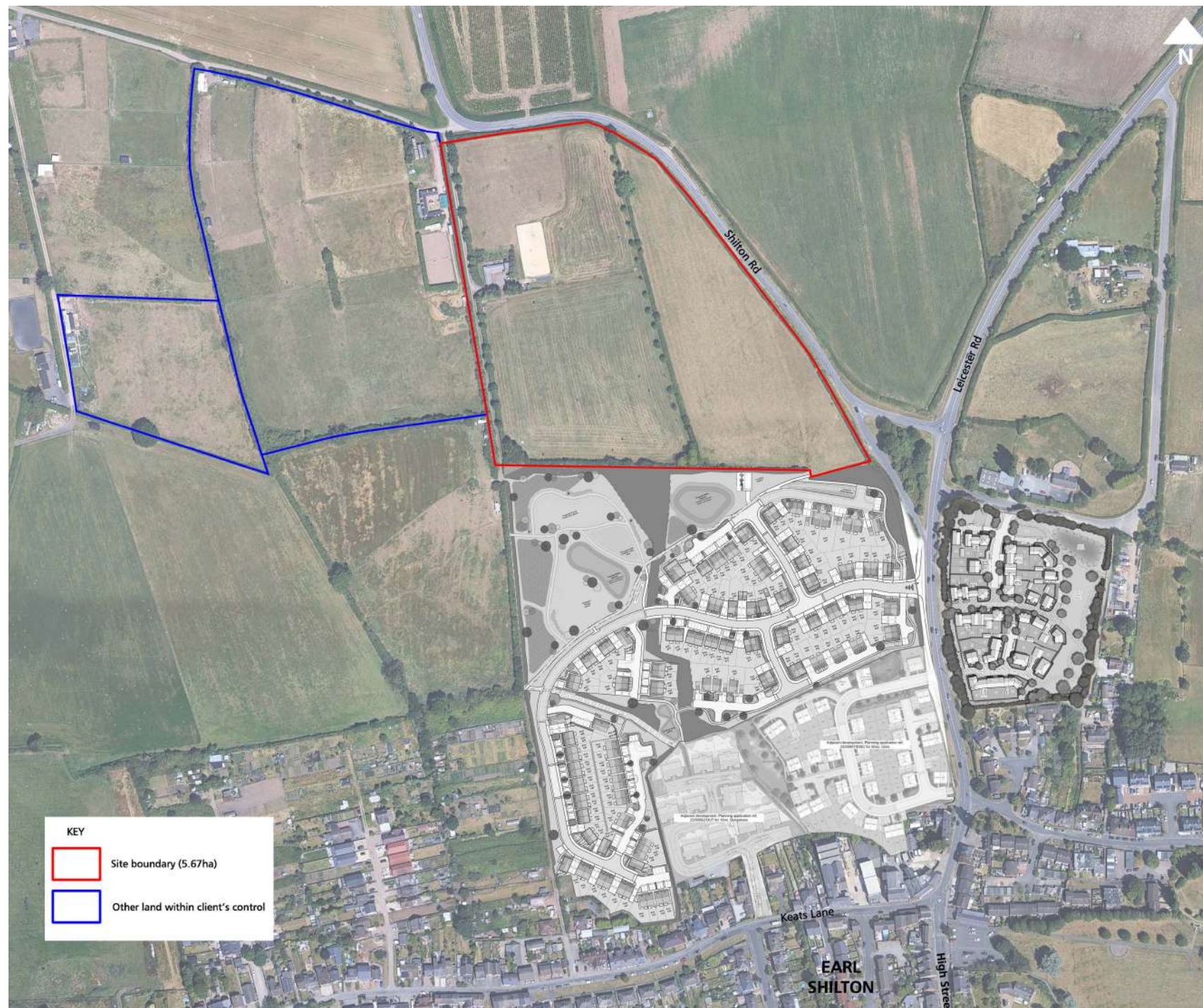


Figure 2: Site Context Plan

Vision and objectives

The site (as outline in red on Figure 2) represents a logical location for the further organic growth of Earl Shilton, following the approval of residential development on land to the immediate south of the site. Shilton Road offers a logical limit to development to the north east. The land outlined in blue is also under the control of the applicant and could offer potential for additional residential development but does not form part of this proposal.

The overall Vision for the site is therefore to achieve a high quality residential environment, where people want to live, in homes that are attractive and energy efficient, close to local services and facilities, allowing residents to make a positive contribution to the local community, whilst respecting and enhancing local character, landscape values and natural habitat.

Key objectives in preparing the proposals have therefore included:

- Contributing to meeting local housing needs, including for those that are unable to access adequate homes within the market (i.e. affordable housing).
- Through this local provision, contributing to the national objective of significantly boosting the supply of new homes.
- Delivery of affordable dwellings.
- Achieving a mixed and balanced community, through the provision of a variety of housing types and tenures that help to meet the diverse needs of existing and future residents, including individuals, couples, families and older people.
- Development that would form an integral part of Earl Shilton, both in terms of its physical form and social and community links.
- Development that would make a positive contribution to the economic vitality of Earl Shilton by way of the new residents having the choice to work in Earl Shilton and having safe and convenient access to local services and facilities that meet their needs.
- Delivery of an inclusive and high-quality development in accordance with best practice.
- Contributing to and enhancing the natural and built environment.
- Promoting healthy and safe communities.

LAND WEST OF SHILTON ROAD, EARL SHILTON

ASSESSMENT

The Site

The site is located off Shilton Road, Earl Shilton, Leicestershire, to the immediate north of the existing edge of Earl Shilton (taking into account approved development), 13km south-west of Leicester City centre. The site, measuring approximately 5.67ha., is comprised of 3 pasture fields, currently used as horse paddocks, with an established hedgerow boundary. A stable and a manege are located at the centre of the site, with a further stable and manege immediately adjacent to the western boundary. There is further agricultural land to the north, east, and west. The land to the south, between the site and the existing urban edge, has been approved for residential development. Currently, vehicular access to the site is taken from Shilton Road, which is the approach route from and linking to Kirby Mallory.

A public right of way (footpath) runs along the western edge of the site.

Earl Shilton has a current population of 10,879 (2021) and supports a range of employment opportunities alongside local services and facilities, including primary and secondary education and primary health care. Once approved development is completed, the urban edge of Earl Shilton will abut the site. Earl Shilton extends to the south/south-west where it joins Barwell.

The photographs on this page illustrate the current character of the site, its generally open and limited slope and relationship with existing urban development.



(Above) View looking south across the site towards the Earl Shilton on the ridge



(Above) View looking south from Kirby Mallory across the valley towards Earl Shilton from Shilton Road

(Below) View from existing footpath looking south towards Earl Shilton showing the topography of the site

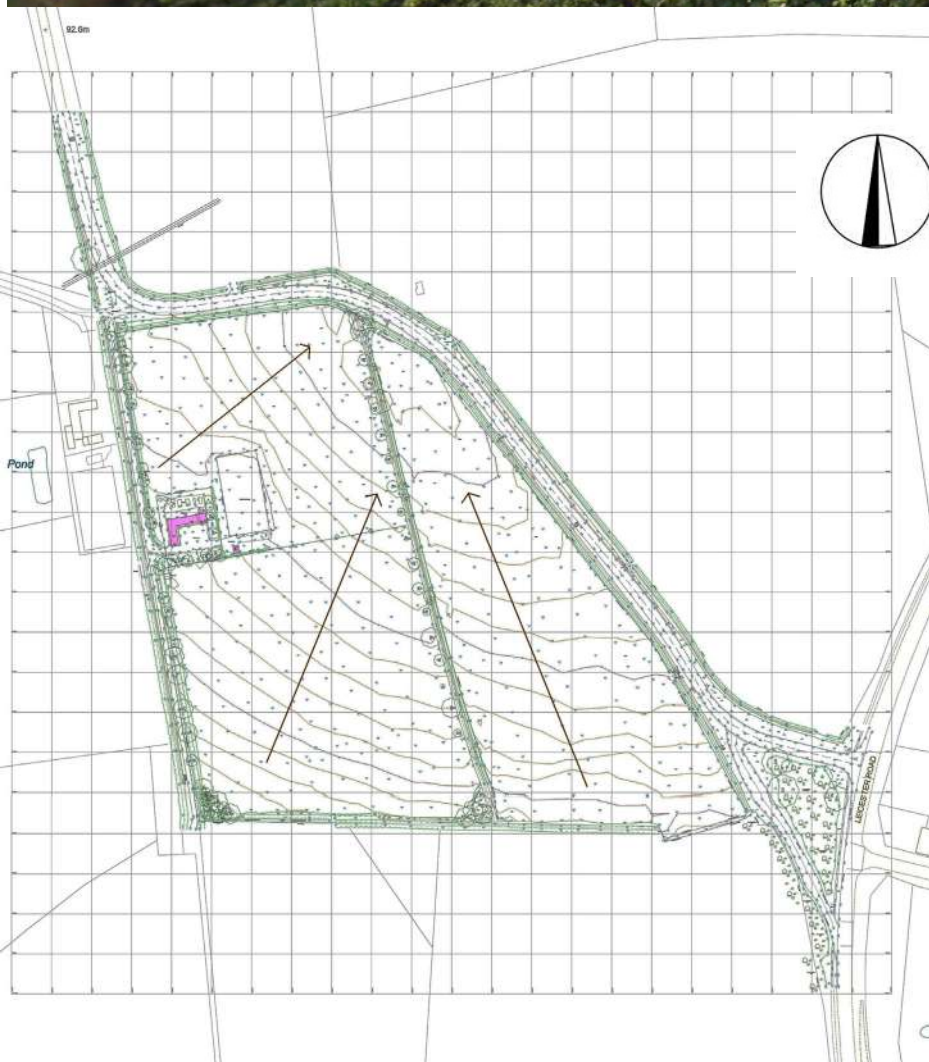


Figure 3: Surrounding landscape and village setting (Google)

Site Context

The site lies on the north eastern edge of Earl Shilton within the District of Hinckley and Bosworth. The Council, within its adopted Local Plan - Core Strategy (2009), has identified Earl Shilton as one of four main urban areas (along with Hinckley, Barwell, and Burbage). The Council consider these urban areas to provide local facilities to their own populations and provide an important critical mass to support Hinckley town centre. As such, they are the focus of most new development in the Borough.

The historic development of Earl Shilton as a linear settlement along a ridge determines its urban character and layout. The central area of Earl Shilton exhibits a local vernacular typical of this part of the East Midlands, shared with other nearby settlements and influenced by local building materials. More recent development expresses a additional variety of design.

Reflecting its status within the Local Plan, Earl Shilton has experienced residential growth in recent years, being recognised as a sustainable location for new development. Figure 4 (below) shows the site in context of existing and approved development, highlighting how the site would represent a further 'organic' expansion of the settlement, whilst remaining proximate to local services and facilities and contained physically and visually by natural and built features of the landscape (road, field boundaries, hedgerow, ridgeline).

Figure 4:
Development site in settlement
edge context



LAND WEST OF SHILTON ROAD, EARL SHILTON

ASSESSMENT

Planning Policy and Guidance

Relevant planning policy and guidance is more fully considered in the Supporting Planning Statement submitted as part of the application. This section therefore focuses on matters of particular relevance to design and access.

The Development Plan

The part of the development plan for Hinckley and Bosworth Borough that is relevant to this proposal, is the Hinckley and Bosworth Local Plan 2006 to 2026, which is comprised of the following:

- Core Strategy DPD (adopted 2009)
- Site Allocations and Development Management Policies DPD (adopted 2016)
- Hinckley Town Centre Area Action Plan (AAP) DPD (adopted March 2011)
- Earl Shilton and Barwell Area Action Plan DPD (adopted 2014)

The Local Plan is being reviewed. It is expected that the replacement Local Plan will cover the period 2024-2045. Following initial exploration of key issues and options, additional ‘Regulation 18’ consultation is, at time of writing, being undertaken, with ‘Regulation 19’ consultation scheduled for around March / April 2026. Whilst the emerging Local Plan provides some insight into the Council’s intentions for meeting the development needs of the Borough through to 2045, the new Local Plan is unlikely to be adopted in advance of a decision being required on this application.

Core Strategy DPD

The Hinckley and Bosworth Core Strategy promotes well-designed, high-quality housing and development by encouraging sustainable, context-sensitive design that reinforces local distinctiveness and settlement identity (Spatial Objective 9), providing an appropriate mix and density of homes that meet local needs (Policy 16), and ensuring developments achieve high levels of energy efficiency and sustainability (Policy 24).

Spatial Objective 9 and Policies 16: Housing Density, Mix and Design; and 24: Sustainable Design and Technology of the Core Strategy DPD can be summarised as below:

Spatial Objective 9 aims to ensure that new development reinforces local distinctiveness, strengthens settlement identity, and enhances the environment through high-quality, sustainable design, helping to foster strong community identity and neighbourhood pride.

Policy 16 requires developments of 10 or more dwellings to provide a mix of housing types and tenures based on local housing needs evidence, achieve a ‘very good’ rating under the Building for Life criteria where viable, and meet minimum net density standards of 40 dwellings per hectare in and around the main towns and 30 dwellings per hectare in

rural settlements, with lower densities only considered in exceptional circumstances justified by site-specific characteristics.

Policy 24 seeks new developments to meet high standards of sustainable design and technology, with residential developments in Hinckley, Burbage, Barwell, and Earl Shilton achieving Code for Sustainable Homes level 6 (from 2016 onwards).

Site Allocations and Development Management Policies DPD

The Site Allocations and Development Management Policies DPD seeks new development to protect the amenity of neighbouring properties, respond positively to the character of the area, use appropriate materials, incorporate high-quality landscaping and sustainable design, and provide safe, efficient, and energy-conscious layouts.

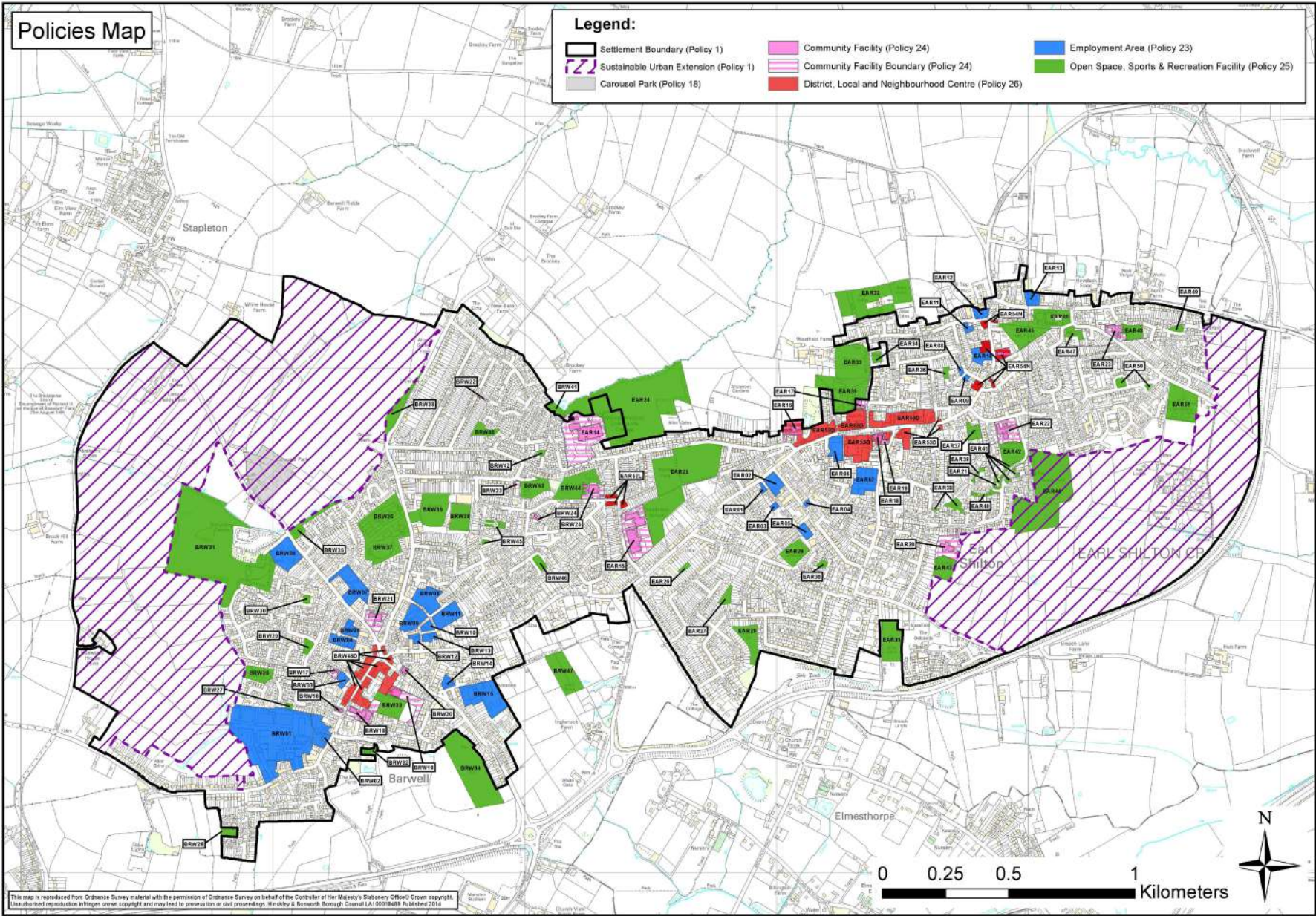


Figure 5: Earl Shilton Policy Plan Extract

1. Context - enhances the surroundings
2. Identity – attractive and distinctive
3. Built form – a coherent pattern of development
4. Movement – accessible and easy to move around
5. Nature – enhanced and optimised
6. Public spaces – safe, social and inclusive
7. Uses- mixed and integrated
8. Homes and buildings – functional, healthy, and sustainable
9. Resources – efficient and resilient
10. Lifespan – made to last

LAND WEST OF SHILTON ROAD, EARL SHILTON

EVALUATION

Services and Facilities

The site is located to the north of Earl Shilton but within easy walking distance using existing footpath links to the town centre and its extensive facilities (see Figure 6 right).

Future residents would benefit the town through support for and use of the existing facilities and services, including those in the shops and other businesses in the town centre. Where improvements in the capacity of local services and facilities is demonstrated to be required, the development will make contributions towards the expansion of those services and facilities. This would include schools

Earl Shilton has a varied retail offer with a range of independent shops and services, a small concentration of national retail chains, banks, post office, health centre, public houses, library and small Co-operative supermarket. All are within safe and convenient reach from the application site. At its closest point, the site is located approximately 0.2km from the shops and services of Earl Shilton town centre. No part of the site would be more than 800m away, which is a suitable travelling distance according to 'Providing for Journeys on Foot' (2000).

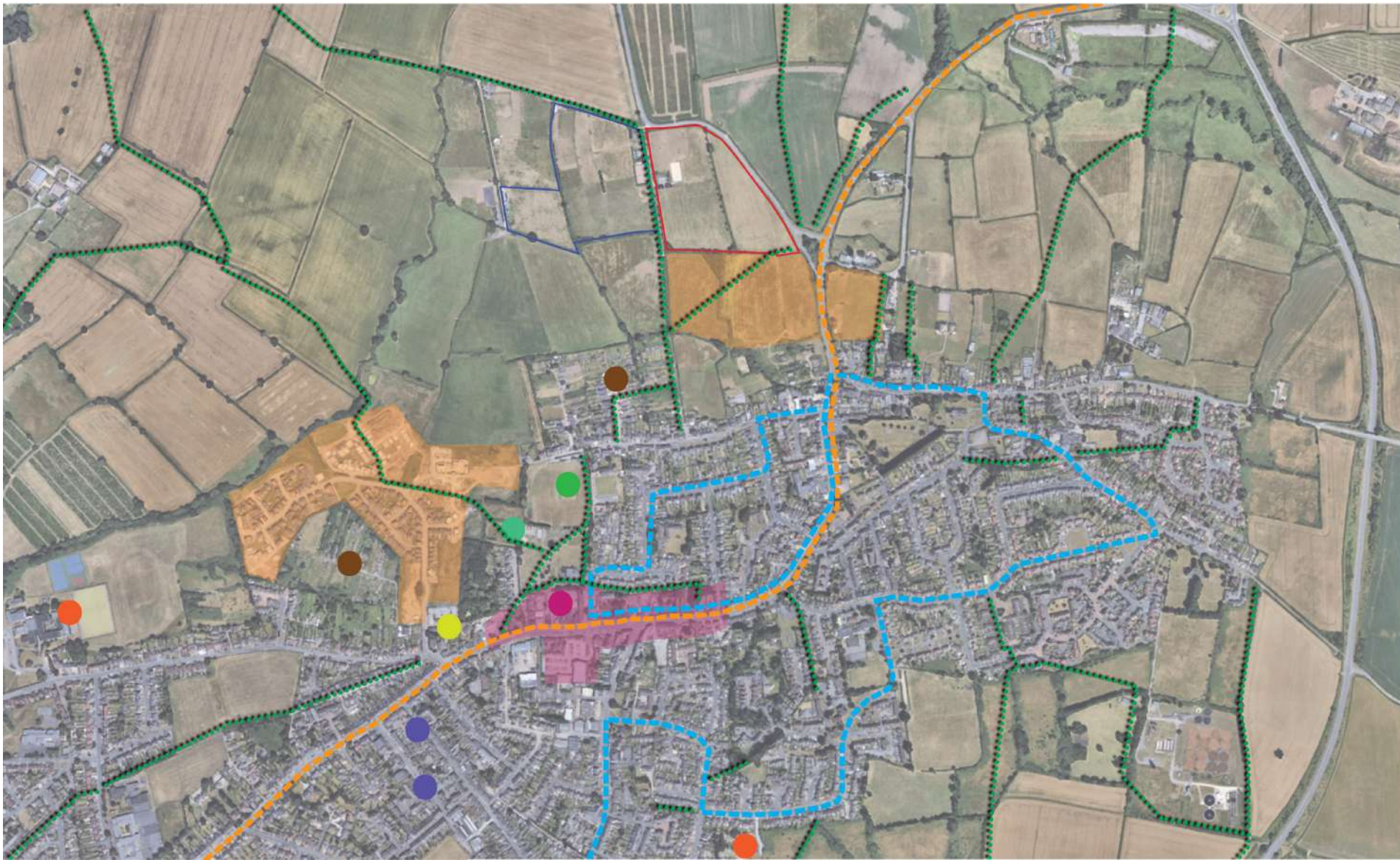
Local schools are also well within acceptable walking distance thresholds, with the William Bradford Community College accessible via Heath Lane, approximately 900m away at the closest point rising up to circa 1.6km. Newlands Primary School is also a 10 minute walk away to the southwest of the site. Other school sites within acceptable walking distance include Heathfield High School (0.7km), Weavers Close Primary School (0.8km) and Townlands C of E Primary School (1km).

There is also good public access to the countryside to the north through an existing network of public rights of way.

Aside to the retail and other services, the town also has an established local employment offer with many businesses based near the centre. This includes The Oaks Industrial Estate, comprising a mixture of small scale starter units, offices, and storage uses.

The town is well connected by local circular bus routes and has frequent wider public transport links to Hinckley, Leicester and Nuneaton.

Figure 6: Earl Shilton Facilities and Services



(Right) The centre of Earl Shilton



KEY

- | | | | |
|--|----------------------|--|----------------------|
| | Site boundary | | Formal recreation |
| | Defined local centre | | Open space |
| | Shops | | Community facilities |
| | Employment | | Bus route 81 |
| | Schools | | Bus route 48 and 158 |
| | Health facilities | | Public right of way |



LAND WEST OF SHILTON ROAD, EARL SHILTON

EVALUATION

Figure 7:
Flood Risk Plan



Topography

The illustrations on this page clearly show the countryside and topographical setting and historic linear ridge line context of Earl Shilton. The elevated aerial viewpoint (above) shows the site against the backdrop of the settlement and ridge. The double bend in Shilton Road delineates the limit of urban expansion.

The site approaching the settlement from Kirby Mallory along Shilton Road, will be screened by mature hedgerows will contain and minimise the visual impact of proposed new housing. The inset plan shows the flood risk areas along the Brook corridor to the north of Earl Shilton. The site is low flood risk.

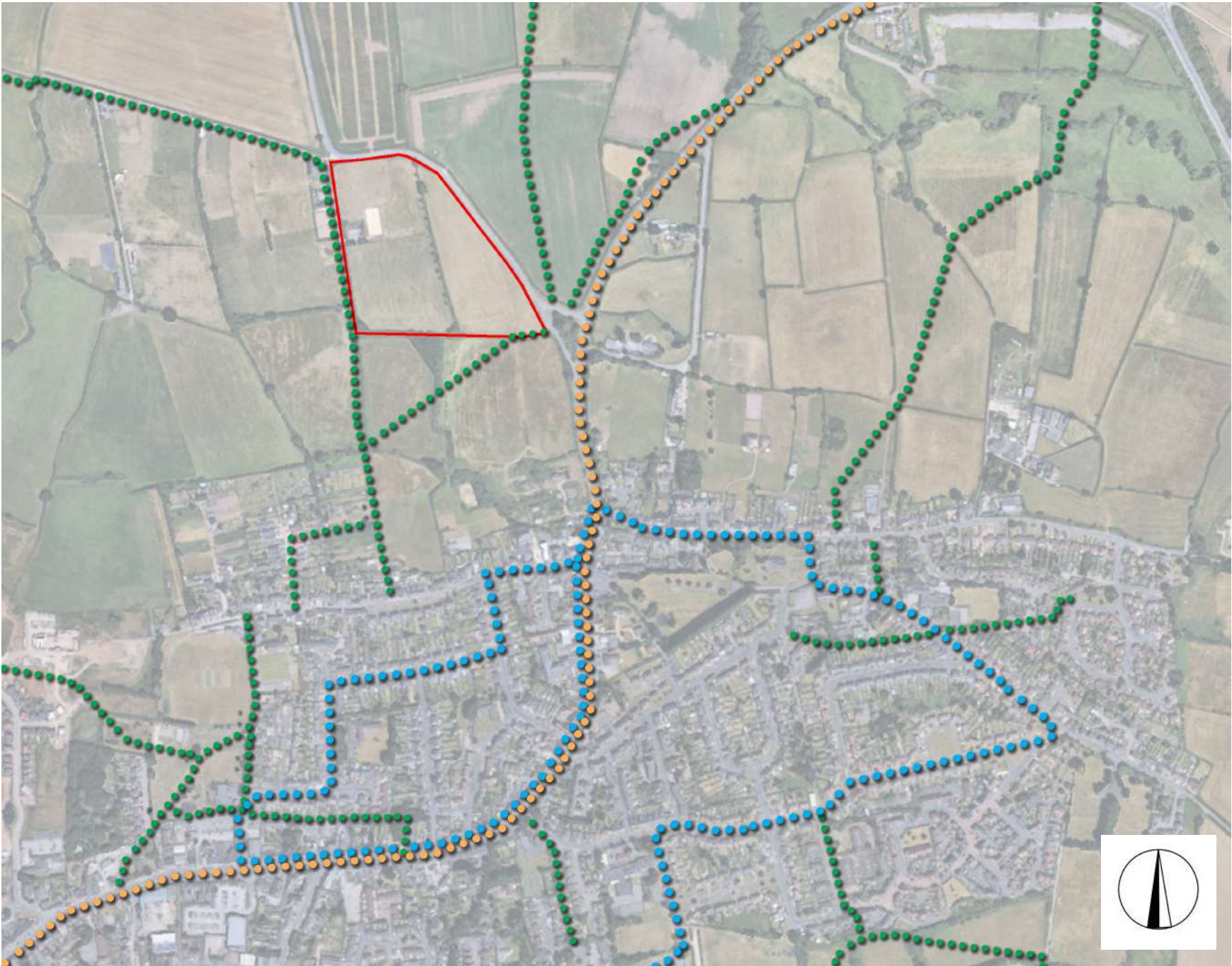


Figure 8: Topography Plan

LAND WEST OF SHILTON ROAD, EARL SHILTON

EVALUATION

Figure 9: Earl Shilton Connectivity Plan



KEY



Site boundary



Public right of way



Bus route 48 and 158



Bus route 81



Above) Good bus connections and access Earl Shilton town centre

Movement and Linkage

The site is well located to take maximum advantage of the potential for use of non-car modes. Served by existing and frequent bus routes 10, 48 and 158 towards Coventry, Nuneaton, Hinckley, Leicester and Barwell and Fosse Park. The 158 operates on a daily basis.

The nearest bus stops are suitably located only a 3 minute walk away from the entrance into the site. With the furthest parts of the development approximately 800m from local bus stops this is compliant with the required distancing from Leicestershire County Highways.

The site is located in close proximity to edge of centre employment areas, enabling future occupiers who work locally in Earl Shilton to avoid the need to travel by car.

Within close proximity to the A47 Earl Shilton bypass, which gives good vehicular travel links towards Leicester and Hinckley / Nuneaton.

There are a good range of town centre car parking facilities, although the existing medical centre and sports and recreational facilities do experience localised congestion at peak times which requires consideration.

Pedestrian routes in the centre of Earl Shilton are established but there is great potential to explore this further to provide new routes and improve signage.

Opportunities and Constraint

The existing public right of way (footpath) along the western boundary of the site is safeguarded. The illustrative masterplan shows how that footpath could be integrated as part of publicly accessible open space providing connectivity towards the centre of Earl Shilton.

- Close association with future edge of Earl Shilton.
- Safe and convenient access to local services and facilities.
- No unusual or significant technical constraints.
- Good visual containment.
- Built development suitable for most of the site.
- Opportunity for drainage infrastructure to be integrated with open space and habitat enhancements.
- Green open space infrastructure to be designed so as to complement and integrate with that proposed as part of development to the south.

This aerial map of Earl Shilton, Leicestershire, illustrates various planning boundaries and proposed developments. A blue outline delineates a specific area in the northwest, while a red outline marks a larger boundary to the east. Two white arrows, each labeled 'Land fill', point to specific locations within the red-outlined area. A blue star is positioned near the top center of the map. The map shows existing residential areas, including 'Earl Shilton Town Cricket Club' in the bottom left and 'Queen Elizabeth II Hall Field' in the bottom right. Two areas are labeled 'Residential development with planning approval'. Key roads include 'Shilton Rd', 'Leicester Rd', 'Keats Lane', and 'High Street'. The map also features contour lines with elevations of 95, 100, and 105. A green star is located near the intersection of High Street and Keats Lane. The text 'EARL SHILTON' is prominently displayed in the bottom center.



LAND WEST OF SHILTON ROAD, EARL SHILTON

DESIGN

Development Parameters

Detailed proposals for the site, including the form and layout of the built development, will need to take account of the input of the community, a wide range of statutory bodies, and of course the Council. At this stage it is therefore inappropriate to present fully detailed and worked out plans.

However, the DAS concept master plan has fully taken on board the need for the development to enhance the visual appearance of the urban edge, to assimilate new development, to create new habitat, to be accessible.

The site has potential to achieve:-

- a community designed in a way that creates a sense of place;
- inclusive layouts which foster a sense of ownership and community safety
- a walkable neighbourhood focused on the needs of pedestrians; housing range and choice;
- landscape protection;
- woodland planting;
- the integration of renewable energies and sustainable materials;
- and, extension which is fully integrated with the existing community of Earl Shilton.

The site lies within close proximity of existing schools, areas of employment, leisure opportunities and local services. New development in this location would help to support and consolidate existing communities and community facilities. In addition, the location of existing education, leisure and employment uses provides for opportunities to expand pedestrian routes and to support environmental enhancements to the town centre. The developers' control of the site ensures that a sustainable scheme is not only realistically achievable, but is also attainable within 5 years of a grant of planning permission.

Although the details of the development are yet to be determined, an initial analysis of the site and the surrounding area suggests a number of key principles will emerge which would shape the form of the scheme, and which would include:-

- A sustainable, locally distinct and energy efficient extension to Earl Shilton;
- A form of development which favours the needs of pedestrians over that of the private car;
- Provision of a choice of sustainable transportation links and modes ensuring excellent connectivity between areas of employment, housing, leisure and shopping;
- The provision of green links with opportunities for enhanced biodiversity and leisure facilities;
- Minimal visual impact and the consolidation and enhancement of existing, trees and hedgerows wherever possible;
- Sustainable Urban Drainage working in harmony with the existing landform.

The structure of the new development will emphasise ease of movement and legibility. Connectivity with the surrounding area will also be of importance. The road network will be designed to efficiently serve the development whilst incorporating treated and combined surfaces to help calm traffic where necessary. The site itself will be made as permeable as possible, with shorter 'cut-through' routes made available to pedestrians.

Landscaping

Hard and soft landscaping is fundamental to the creation of high quality places, establishing site boundaries, breaking up the mass of development, screening and to assimilate the development within the countryside contextual setting. Existing hedgerows and open space features will be fully integrated and enhanced as part of the layout design, providing a landscape edge 'buffer' to the countryside and a series of linked green open space corridors linking into the town centre..

Different open space types will be provided across the site and adding spatial interest, place making and enabling different functional types of open space for residents to enjoy. Existing footpath green corridors will be part of the green infrastructure

Access and Parking

The proposals include a new priority junction from Shilton Road, providing a 6m wide residential road with 6m radii, local widening of Shilton Road to maintain a 6m carriageway, and visibility splays of 2.4m by 203m to the north and 2.4m by 103m to the south, exceeding the required standards. A 3m dual-use pedestrian and cycle facility is also proposed along Shilton Road..

Appearance

Although details of appearance are reserved for future consideration at the Reserved Matters stage, consideration of the neighbouring site and local vernacular of Earl Shilton as a whole will be taken into consideration. The development will enhance the character of the surrounding area by using appropriate building materials that reflect the local vernacular, promote high standards of landscaping and conservation of energy, and a design that promotes natural surveillance, including fire safety measures and will seek to maximise the principles of secured by design. Building materials mostly comprise red brick with some neutrally painted render to reflect the colour and texture palette for Earl Shilton.

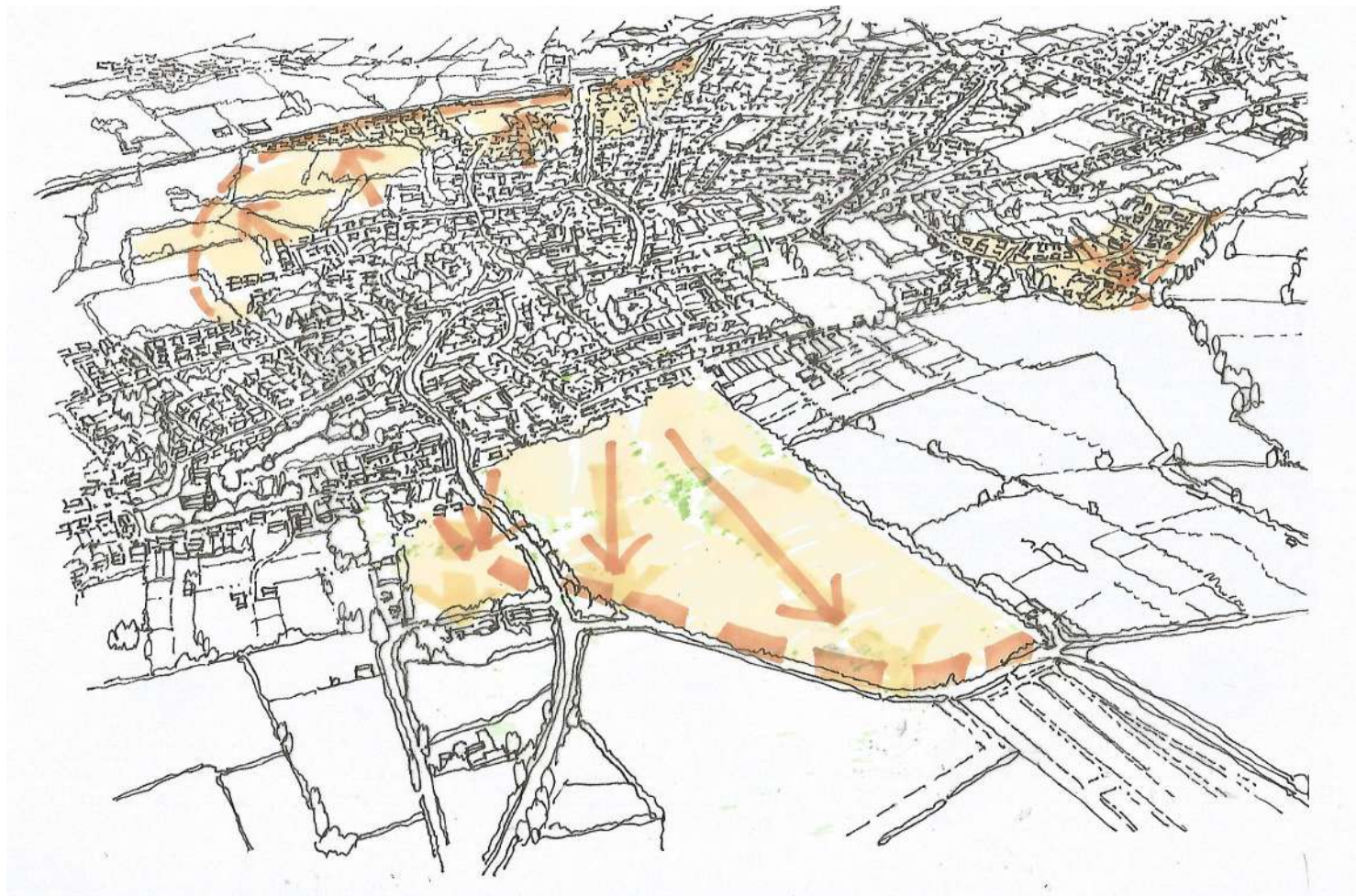


Figure 11:
Earl Shilton Urban Morphology
and potential for growth

The diagrammatic aerial perspective on this page clearly illustrates the potential for urban expansion to Earl Shilton.



Use and Amount

The development can deliver up to 120 dwellings of which 20% would be Affordable Housing. In addition, the proposal will accommodate amenity open space, urban parkland, direct access to the Countryside, access and parking and connections to existing Public Rights of Way.

There is an opportunity to create a network of green open space corridors within the development fully integrating and linking the urban form with its landscape and topographical setting.

The proposed development is of sufficient size to create a new residential offer and new neighbourhood.

Scale and Density

New development on the site will take a cue from the built tradition of this part of Leicestershire, with building predominantly 2 storey. The built form could be 2.5 storey to provide focal features in key townscape locations in the higher density parts of the development.

The site is to accommodate a density of 40 dwellings per hectare. Density will vary across the site, with a lower density at the countryside edge, to achieve an appropriate interface with landscape character.

Restricting the height of the built form will ensure that the development will sit below the skyline of the ridge and visually be seen as an integral part of the existing urban envelope in character with Earl Shilton's image as a hill top settlement.

Character

The development scheme will follow the principle of perimeter block form with housing frontage onto the street space. The development will be more urban in character towards the town centre and focus of the development.

It is important that good townscape and urban design principles be applied to the detailed design for this site to create a visually interesting development with a hierarchy of quality, well designed and detailed public realm.

The development should match the building palette of Earl Shilton and this part of Leicestershire – predominantly red brick, tiles and slates, with some elements of painted render.



LAND WEST OF SHILTON ROAD, EARL SHILTON

DESIGN

Design Evolution

This scheme seeks to maximise the capacity of the site, making best use of available land and boosting the supply of housing; true to the unequivocal principles and objectives of the NPPF. The illustrations on this page show the wider urban expansion opportunity along the northern edge of the settlement within the parameters of Shilton Road .

The current Westfield Farm development and tthe two existing housing sites - Permission A1 - 24-00496-REM (Vistry Group)
Application A2 - 24-00484-OUT - (Manor Oak Homes) sets a precedent for developing housing on this edge of Earl Shilton. The applicants Site B is a logical extension to site A.

- The potential phasing sequence is:
1. First phase - expansion rear of Keats Lane A1 and A2
 2. Second phase - further expansion to Shilton Road Site B

The illustration below shows the potential new urban envelope for Earl Shilton. Following on from this, the indicative masterplan for Sites B and C has been worked up in further detail. on page 14.

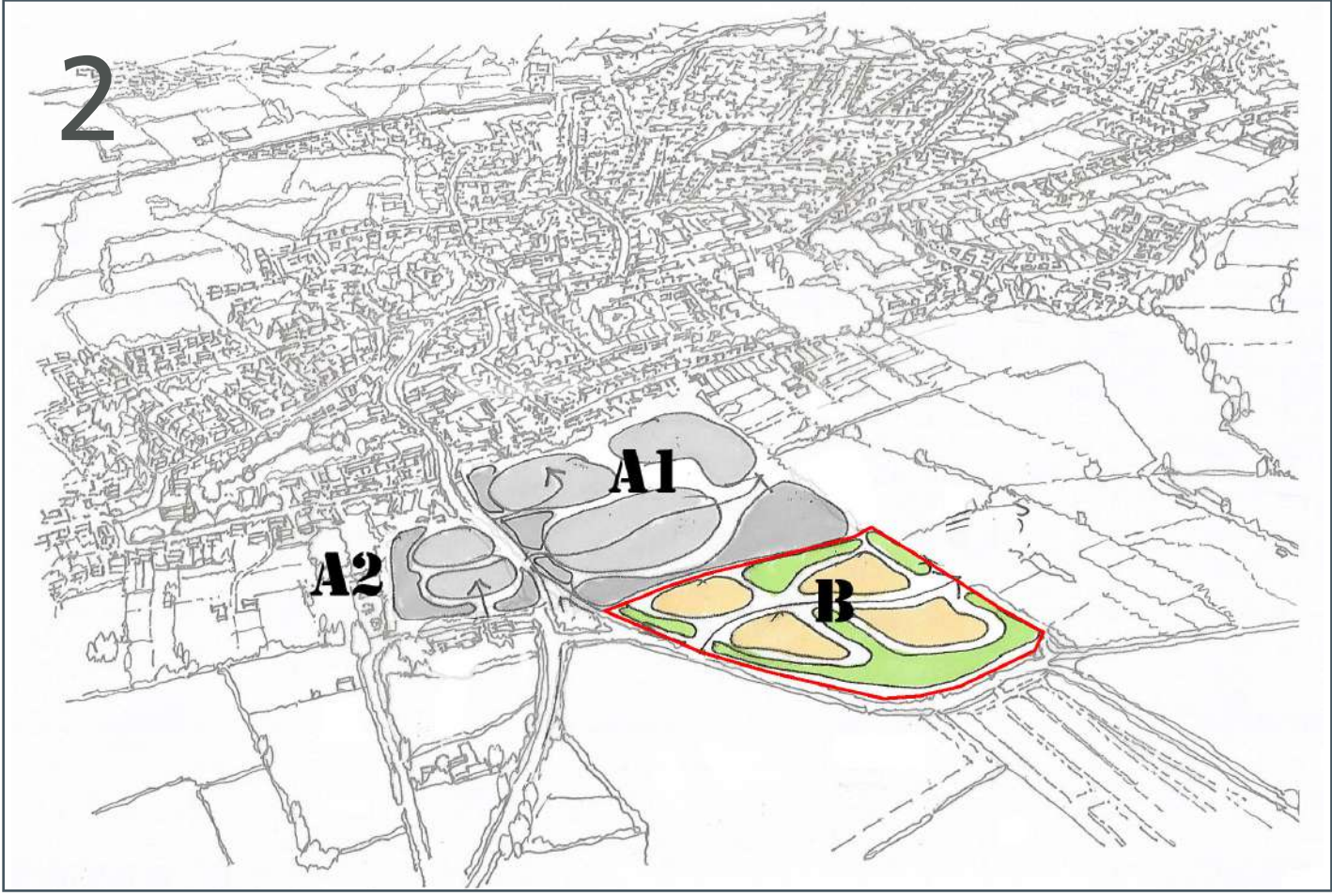
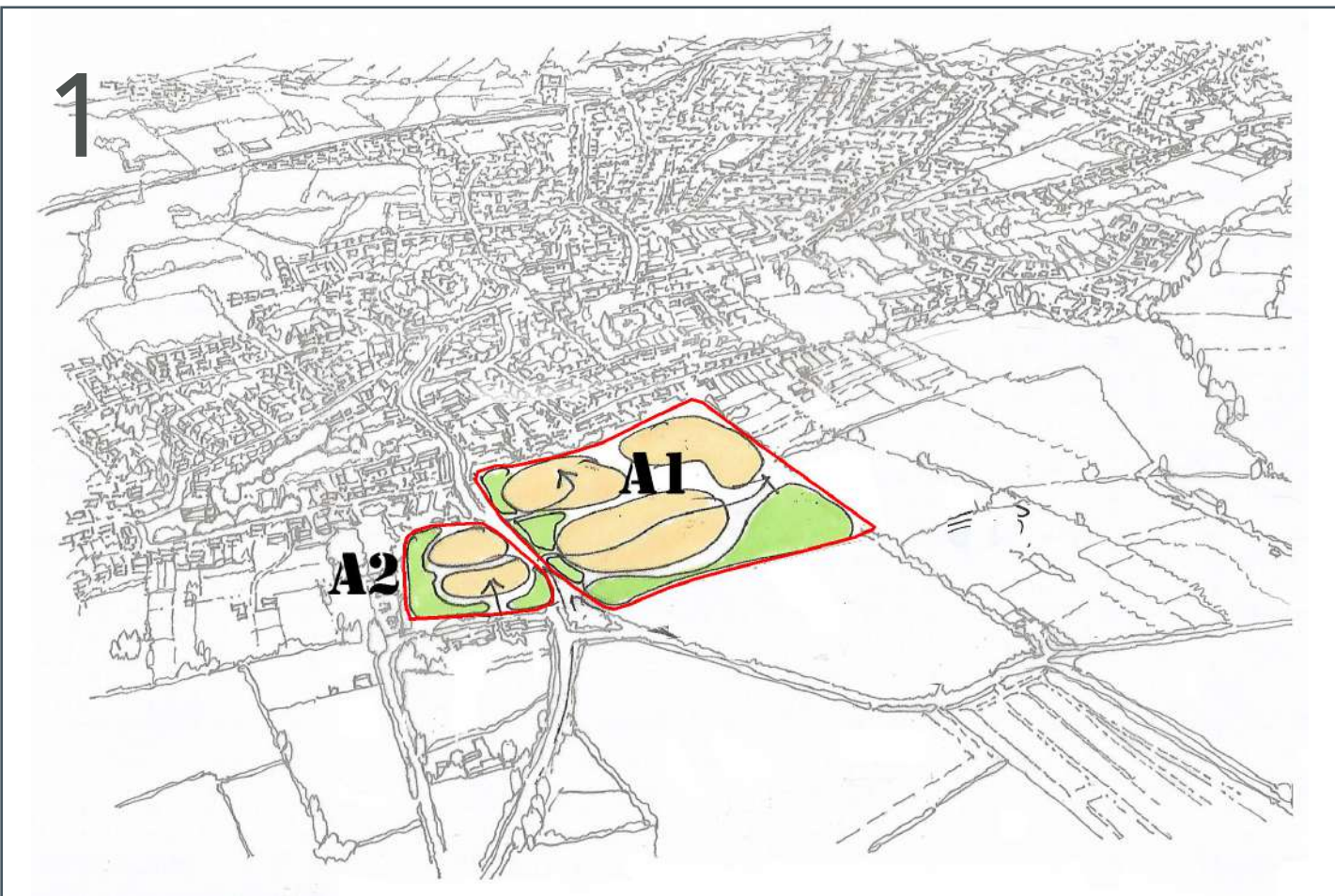












Figure 12: Earls Shilton Northern Edge Sustainable Urban Extension



Figure 13: Site Layout Plan

- KEY**
-  Site boundary
 -  Other land within client's control
 -  Proposed residential development
 -  Existing pedestrian route
 -  Existing bridleway
 -  Proposed pedestrian route
 -  Existing vegetation to be retained
 -  Proposed vegetation
 -  Proposed primary vehicular route
 -  Proposed minor / shared surface vehicular route
- 1** Existing vegetation to be retained and enhanced where possible. Proposed vegetation to enhance character and potential for SuDS location in lower ground.
 - 2** Proposed pedestrian routes along green spaces overlooked by proposed dwellings, creating safe and attractive routes. Potential for pedestrian connections to existing pedestrian route, bridleway and adjacent development.
 - 3** Proposed vehicular access from Shilton Road and highways improvements, including provision of pavement to western / southern side of Shilton Road.



LAND WEST OF SHILTON ROAD, EARL SHILTON

DESIGN

Development Principles

This section describes the proposals and sets out key principles that have informed the evolution of their design.

At the beginning of the design process the key concepts and principles behind the development are set out. These principles have informed the choices behind the amount, layout, scale and appearance of the development and the way that it is accessed, landscaped and constructed. The key development principles are as follows:

- **Creating a sense of place** – the proposed development will have its own distinctive character. The philosophy has been to seek quality solutions, setting high standards of design. A common language of façade treatment, roof finish and material use will unite the variety of buildings on the site to determine development character as one place. Attractive landscaping will complement the buildings and the spaces around them, reinforcing the feel of a robust and quality development.
- **Creating continuity and enclosure** – where possible, access roads and footpaths will be fronted and enclosed by built form. Where this is not possible, appropriate landscaping will define and enclose spaces at a human scale. Careful use of hard and soft landscaping will mark clear distinctions between public and private outdoor spaces. All spaces within the site will fall into a specific ownership ensuring that no leftover spaces will be unused and uncared for.
- **A quality public realm** – public spaces and routes within the development will be attractive and welcoming to the user. Hard and soft landscaping will be carefully detailed and integrated with style and materials of each building. Well-designed landscaping, lighting and street furniture will create a place that is safe and accessible for people of all ages and abilities. Consequently, all spaces and routes will be uncluttered and easily maintained.
- **Good connectivity** – the development will be designed so that it is easy to get to and move through. The entrance to the site is close to existing footpaths and bus services, therefore the layout has been designed to provide direct connections to this existing movement network. Each part of the site will be easily accessible via a choice of direct, high-quality routes.
- **Ease of understanding** – the treatment, scale and location of buildings within the development will aid orientation and allow people to easily find their way. Prominent buildings at the entrance will mark the gateway into the development. Views to key buildings will be possible from within the site and from long distances. Links around the site, particularly for pedestrians will be highly navigable and well-lit.



STREET

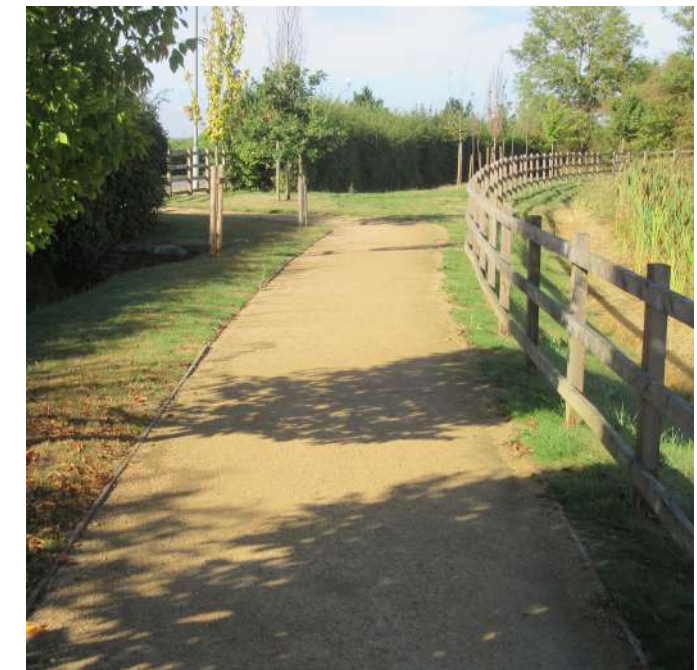


PERIMETER BLOCK



COUNTRYSIDE EDGE

Quality surface treatment and materials



Development Principles

Façade and interface – the relationship between the public spaces and buildings within the development will be important. The architectural expression of individual buildings, entrances, corners, roofscapes and projections will help to interpret its use and status. The nature of each building's setbacks and boundary treatments will also help to define public and private spaces.

Details and materials – a common design language will be used throughout the development in order to promote a sense of identity. A theme of specific architectural elements and features will provide visual links between each building. A palette of soft landscape elements, hard finishes, street furniture and lighting will be used throughout, providing continuity. A distinct selection of materials will be used with a suitable agreed mix of colour and texture. All materials will be naturally durable or treated so as to promote maintenance free longevity. Where possible, materials should be responsibly sourced.

Streetscape – the design of routes and spaces will add quality to the development. Paving, planting and street furniture will be of a high standard. Lighting and signage will be well integrated into both buildings and the public realm. Simple, durable fittings will be specified that provide a high degree of functionality whilst complementing the surrounding architecture. Attractive and appropriate lighting will be placed on buildings, along key routes and in public spaces to aid orientation and contribute towards safety. Appropriate planting will also help to shield from wind and rain as well as provide shade from the sun. All well used ground surfaces will also be treated where required to avoid slipping. Long term management and maintenance of all of these issues will also be considered from the outset.

The streetscape will be positive; with a rich and changing street frontage, positive key buildings turning corners. The curve of the access road will be reflective of a traditional street and will ensure street enclosure.

The interface between buildings, variety of materials and integrated planting, will create a well-designed tapestry and street scene.

A traditional street and a well designed public realm increases the opportunity for social interaction and also a chance for community interaction, wellbeing, neighbourliness and natural surveillance, ultimately 'building for a healthy life'.



Creating good streetscape and frontages

LAND WEST OF SHILTON ROAD, EARL SHILTON

DESIGN

Landscape and Boundary Treatment

The proposed surface water attenuation and landscaped area is located within lower part of the land, adjacent to Shilton Road. Residential development is contained within the backdrop to hillside to the south. This is important due to the topography of the site, creating a visually interesting roofscape and broken views into the site reducing visual impact of the proposed housing.

Visual prominence and wider influence on the surrounding landscape is minimised through the layout and scale of development, supported by retained boundary hedgerow, edge landscape buffer with new tree planting and greening in gardens, and internal open space corridor.

The site is capable of accommodating the proposed development within the capacity of the landscape. The level of landscape impact falls below 'significant'.

Further details of retaining features follow in this Design and Access Statement.

(Right)
Existing hedgerows and trees to be retained as integral part of the green infrastructure of the masterplan



(Above)
Existing footpath links and green edges safeguarded and fully integrated into the new green infrastructure of the site proposal.



Figure 14: Proposed Green Infrastructure



Drainage

The proposed drainage strategy is based on connecting to the existing surface water drainage network at restricted rates. The sites drainage flow proposes run off into the existing brook to the north. On site provision is controlled by a Sustainable Drainage System (SuDs) to temporarily accommodate excess surface run off and to alleviate flood risk ifurther down the natural drainage system.

Foul drainage from the development will connect into the existing sewer system. The illustration below shows an indicative drainage strategy for development expansion to the north of Earl Shilton.

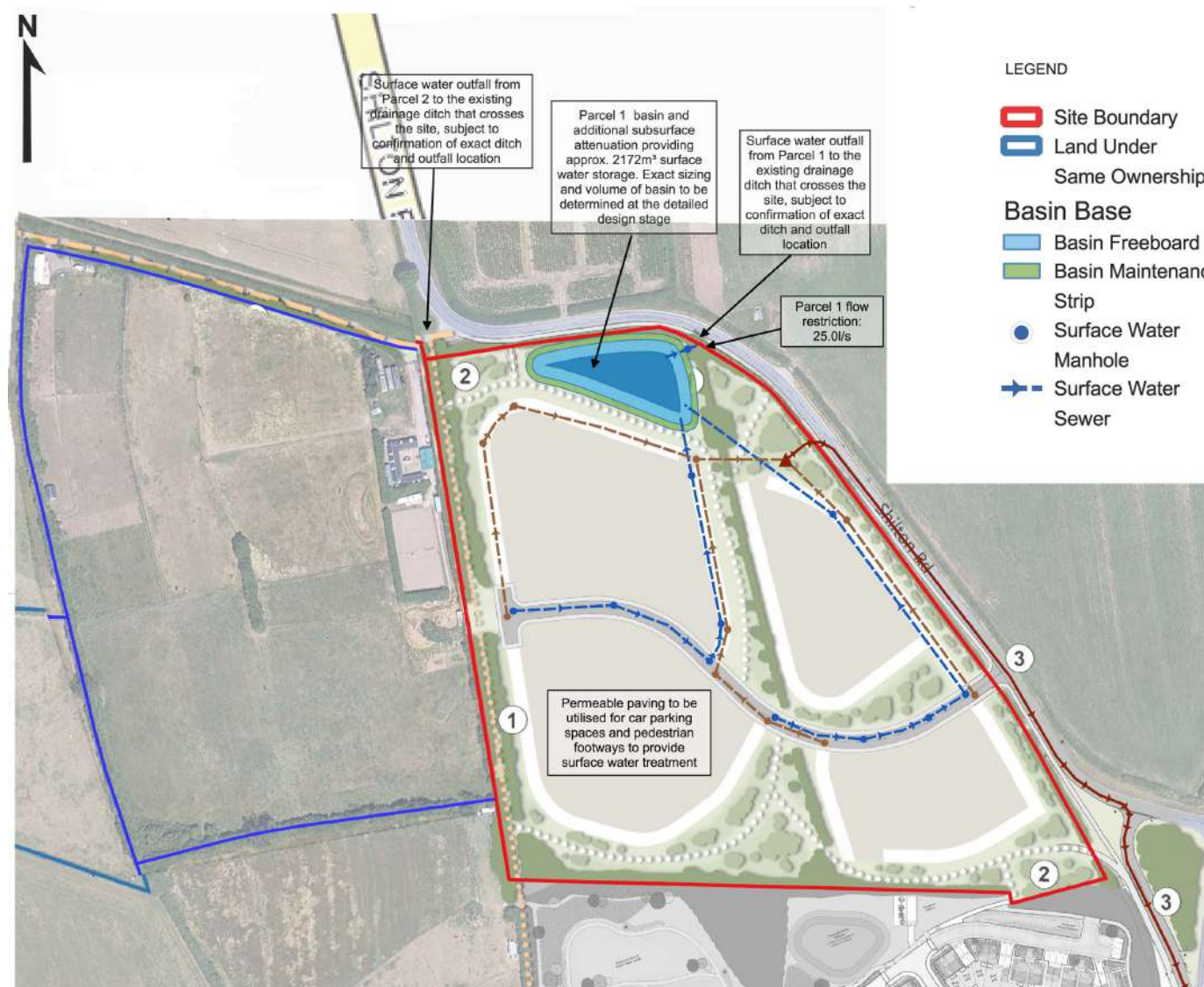


Figure 15: Drainage Strategy Plan

Ecology and Biodiversity Net Gain

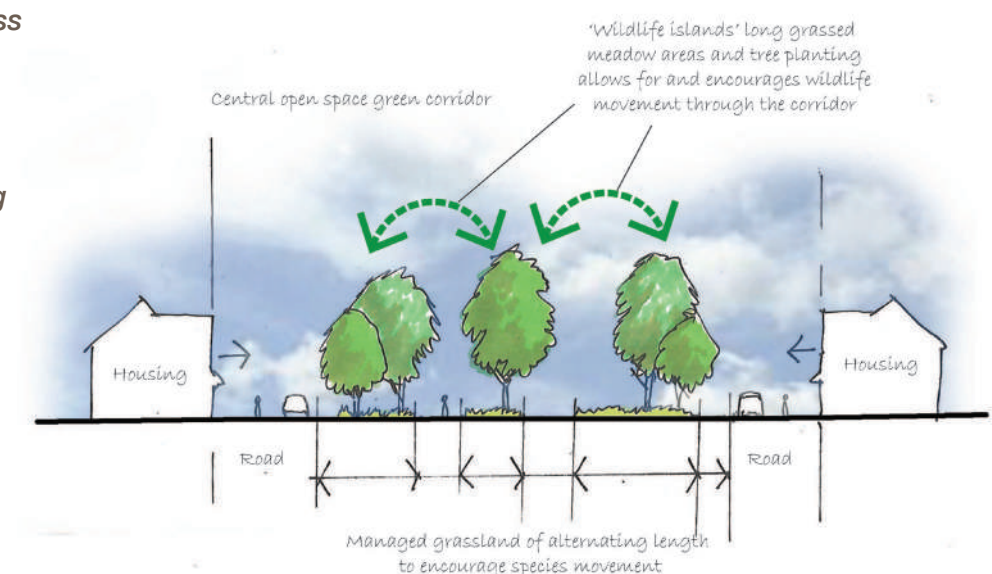
The proposed green infrastructure will allow for a series of linked green 'wildlife' corridors penetrating the proposed urban fabric with direct link to the adjacent countryside. Wildlife diversity will be encouraged by:

- interface countryside edge buffer with new wetland habitat;
- safeguarding maintaining and reinforcing existing hedgerows;
- new woodland and tree planting 'pockets';
- new linear green wildlife friendly open space corridors linking existing open space within the urban envelope, and;
- providing permeable 'green' garden network with wildlife friendly boundaries.



(Above right)
Existing hedgerows across the site will contribute towards bio-diversity

(Right)
Generic diagram showing how tree planting and green open space within the development can for wildlife friendly ecology corridors



LAND WEST OF SHILTON ROAD, EARL SHILTON

DESIGN

Built Form and Perimeter Block Design

The main access road into the site will reflect a traditional 'village street' with an informal grouping of buildings with a linked wall frontage boundary definition. The winding access road will reflect the character of a curved village street space. Built form will reflect the vernacular tradition of traditional street morphology and the houses and garage blocks reflecting good townscape principles.

The aerial perspective (right) shows the varied street grouping and is indicative of proposed character of a typical residential perimeter block. The street frontage is continuous with built form defining and overlooking the street. The frontage has a set back building line with private frontage interface with the public street. The internal space to the block is private gardens and defensible space. This also illustrates the significance of green space within the block adding to biodiversity.

Countryside Edge Design

The illustrations on the facing page show the opportunity to create an open space 'buffer' to the development. This will be an important interface transition and soften the edge of the development with the countryside. This shows enhanced woodland edge with SuDS wetland habitat at the lowest point of the site. The landscape buffer also provides for amenity open space for informal public access and use.

Single aspect lower density housing built frontage facing onto private drive culs-de sac will overlook this space.

The inset perspectives are indicative artistic impressions of the character of this edge.



(Above)
Indicative example of perimeter block development

STREET FRONTAGE AND RETAINED HEDGEROW SPACE DEFINITION



The illustration (left) is an indicative artistic impression of housing frontage fronting onto a tertiary 'homezone' residential road with shared surface cycle and footpath linkage along the line of an existing enhanced hedgerow.

INTEGRATED COUNTRY SIDE BUILT EDGE



The illustration (above) is an indicative artistic impression of proposed single frontage tertiary 'homezone' residential road overlooking open parkland with SuDS pond. This defines the countryside edge of the proposed development.



(Above)
Indicative edge of development

COUNTRYSIDE OPEN SPACE



The illustration (right) is an indicative artist impression of the footpath link from Shilton Road through the green open space parkland toward the set back proposed new housing. This provides a distinct landscape 'buffer' screening and containing the new housing in the landscape.

LAND WEST OF SHILTON ROAD, EARL SHILTON

DESIGN

Materials and Details

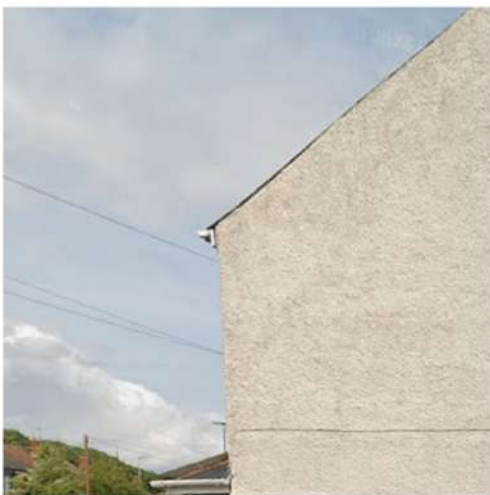
Earl Shilton is typical of the villages found in south Leicestershire and the East Midlands. The building tradition of this area is a predominantly red brick and neutral painted render.

Red brick has been traditionally used in nineteenth century buildings in the settlement (see terraced housing). Some buildings are rendered and usually white painted or neutral colour finish.

Free standing wall, hedgerow and railing boundaries are a feature found in Earl Shilton. Examples on this page shows this range of frontage boundary treatment including red brick with blue brick coping commone to nineteenth centure urban street frontage.

Roofing material is predominantly blue welsh slate or buff/red plain tiles with 45 degree pitch roofs.

The illustrations on this page reflect this building palette and should be used to inform appropriate building material colour and textures. The illustrations also visualise Earl Shilton building scale, proportions, roof detailing and the importance of chimneys to the skyline silhouette.



(Right)
An indicative materials and details palette
suitable for new development in Earl Shilton

Designing out Crime and Ensuring Community Safety

The layout will incorporate design features to prevent crime and to ensure the safety and well-being of future residents. The orientation of dwellings will be carefully considered to maximise natural surveillance. Habitable rooms with an increased number of windows overlook public spaces (footpaths). Blank walls and recesses can be avoided within the design of the dwellings to avoid any areas being out of view.

The mix of dwelling types and sizes will encourage a mix of occupants having different patterns of activity and employment. This will provide an opportunity for natural surveillance for longer periods across the development.

Sufficient off-street parking is provided for security purposes and to avoid on-street parking improving highway safety.

To further deter crime, the development will be well lit through street lighting, illuminated to BS5489 standards. The features incorporated within the design are proportionate the scale of proposed development and the rural nature of the village.



Sustainability

Social

- The proposed development will increase housing choice and availability in Earl Shilton
- Support the vitality of Earl Shilton town centre
- Opportunities for crime and anti-social behaviour are prevented through the design of the proposed development, creating overlooked public spaces (footpaths), private parking and well-lit

Economic

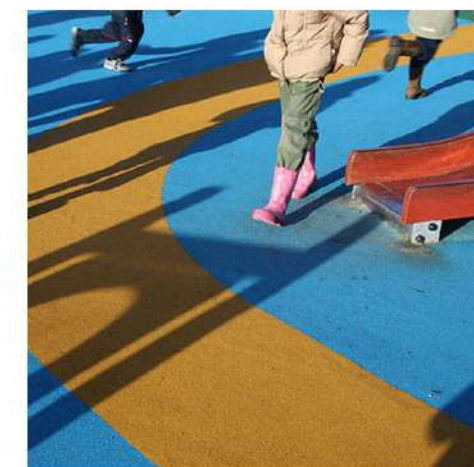
- Employment during construction and use of local materials, where possible
- Increased local spending
- New homes bonus

Environmental

- The proposed development will seek to protect, maintain and enhance areas of biodiversity by retaining and reinforcing boundary planting
- Introduce new areas of soft landscaping
- Making the best and most efficient use of land for development

Sustainable Construction

Building technology is moving quickly and given this application is made in outline, we are not able to specify construction details at this stage however, the scale and location of the proposed development presents an opportunity to build in a high level of energy efficiency, modern building techniques and use of local materials.



LAND WEST OF SHILTON ROAD, EARL SHILTON

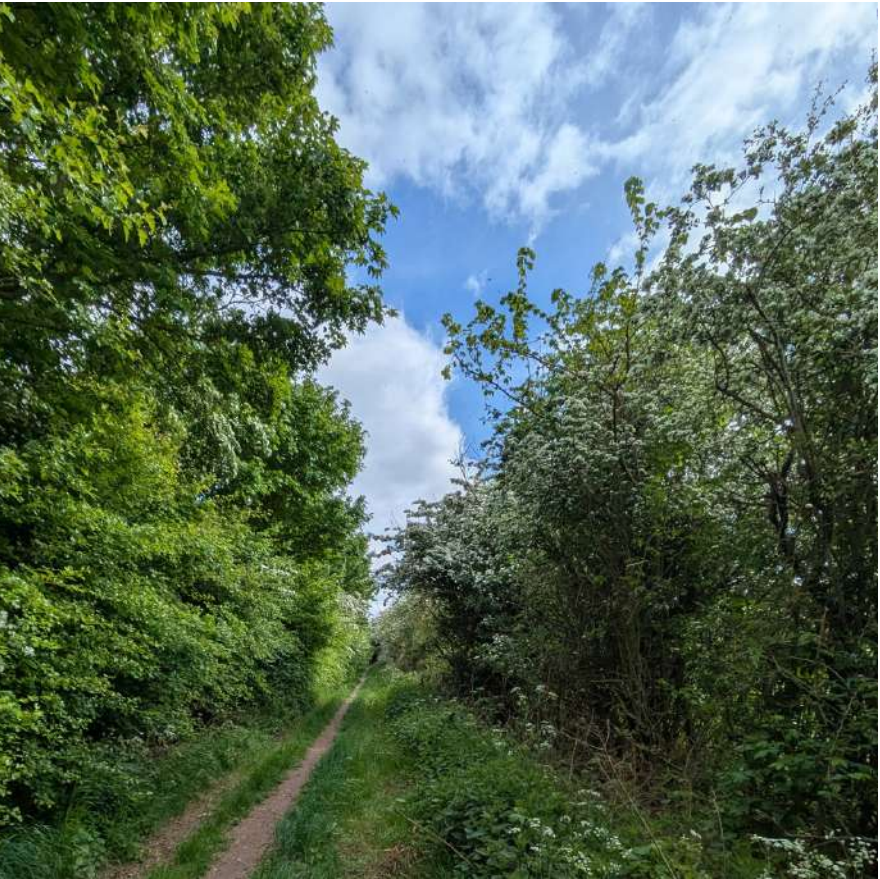
ACCESS

Access

The illustrative layout and developable areas, including the highway hierarchy, have been designed with reference to the Council's Good Design Guide and National Design Guide (2020). The detailed layout and appearance will be confirmed at the Reserved Matters stage, however, submitted drawings indicate the principles for development on the site.

The proposals include a new priority junction from Shilton Road, providing a 6m wide residential road with 6m radii, local widening of Shilton Road to maintain a 6m carriageway, and visibility splays of 2.4m by 203m to the north and 2.4m by 103m to the south, exceeding the required standards. A 3m dual-use pedestrian and cycle facility is also proposed along Shilton Road.

Off-site improvements include a new speed limit gateway, realignment and narrowing of the Shilton Road slip lane, possible provision of a bus stop cage (see accompanying Transport Statement), and enhancements to existing public rights of way including uncontrolled crossings ensuring the site is well connected to Earl Shilton. The site benefits from good access to sustainable travel modes, with nearly all of Earl Shilton accessible within 2km walking distance and the nearest bus stop within 600m, alongside a proposed future stop 65m from the site access.



(Above)
Existing public rights of way will be safeguarded and integrated into the proposed layout and new footpath and cycle routes will provide a network of linkage within the green open space infrastructure across the site.

Figure 16: Access Context Plan

(Below)
Vehicular access into the site will be from Shilton Road. This will be in a proposed new 30mph zone on approach to the junction with Leicester Road (see Vehicle Access Detail Plan on page 24).

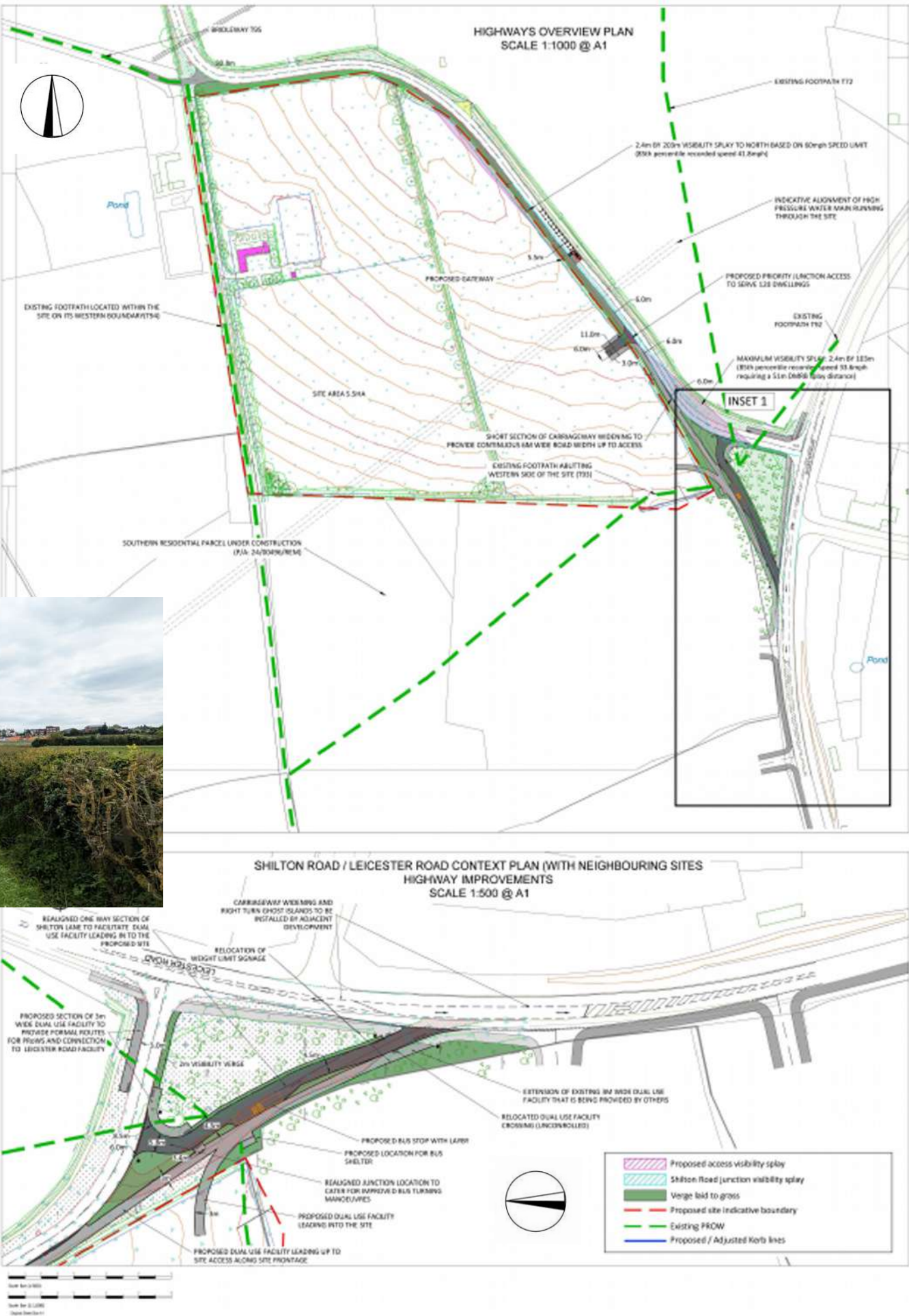


Figure 17: Vehicle Access Detail Plan

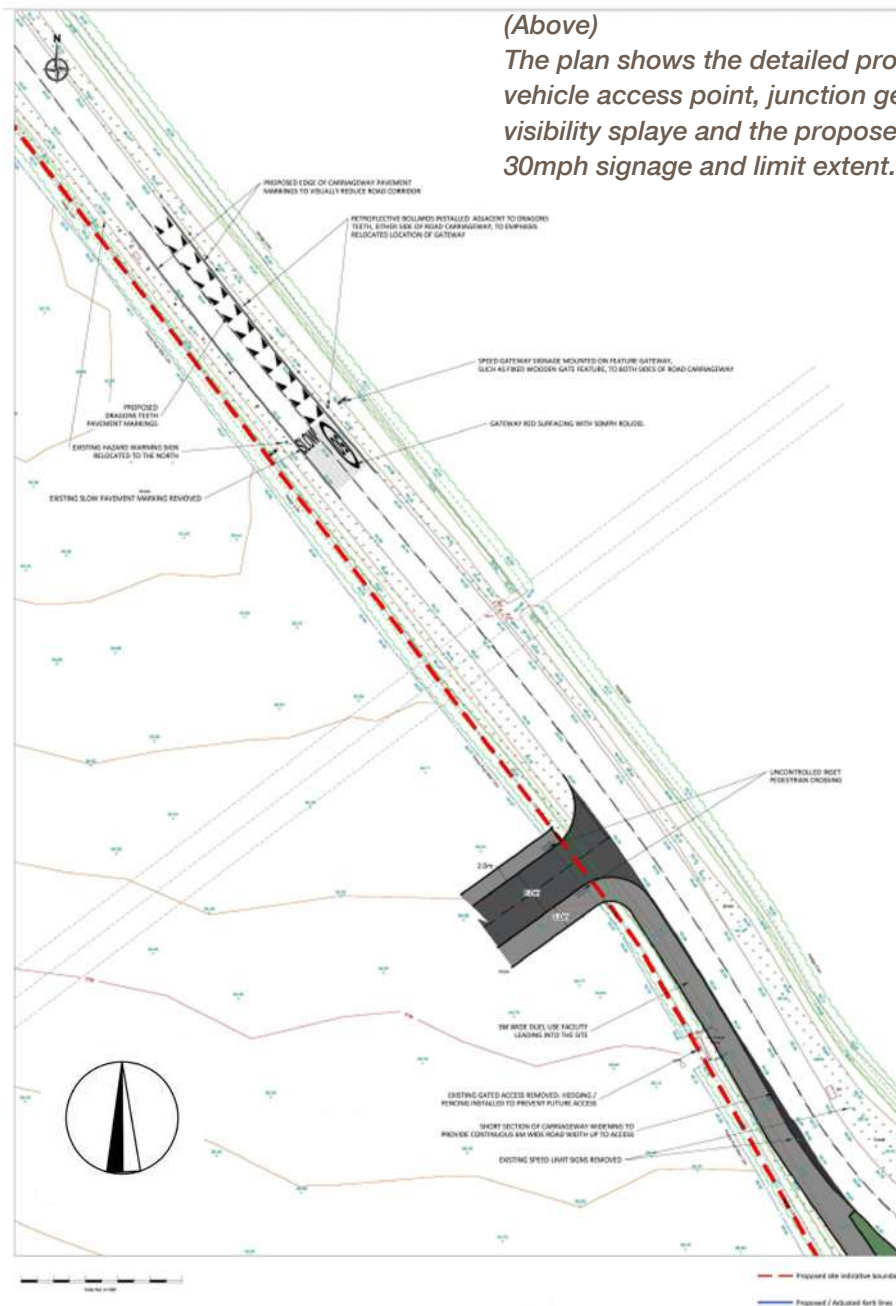
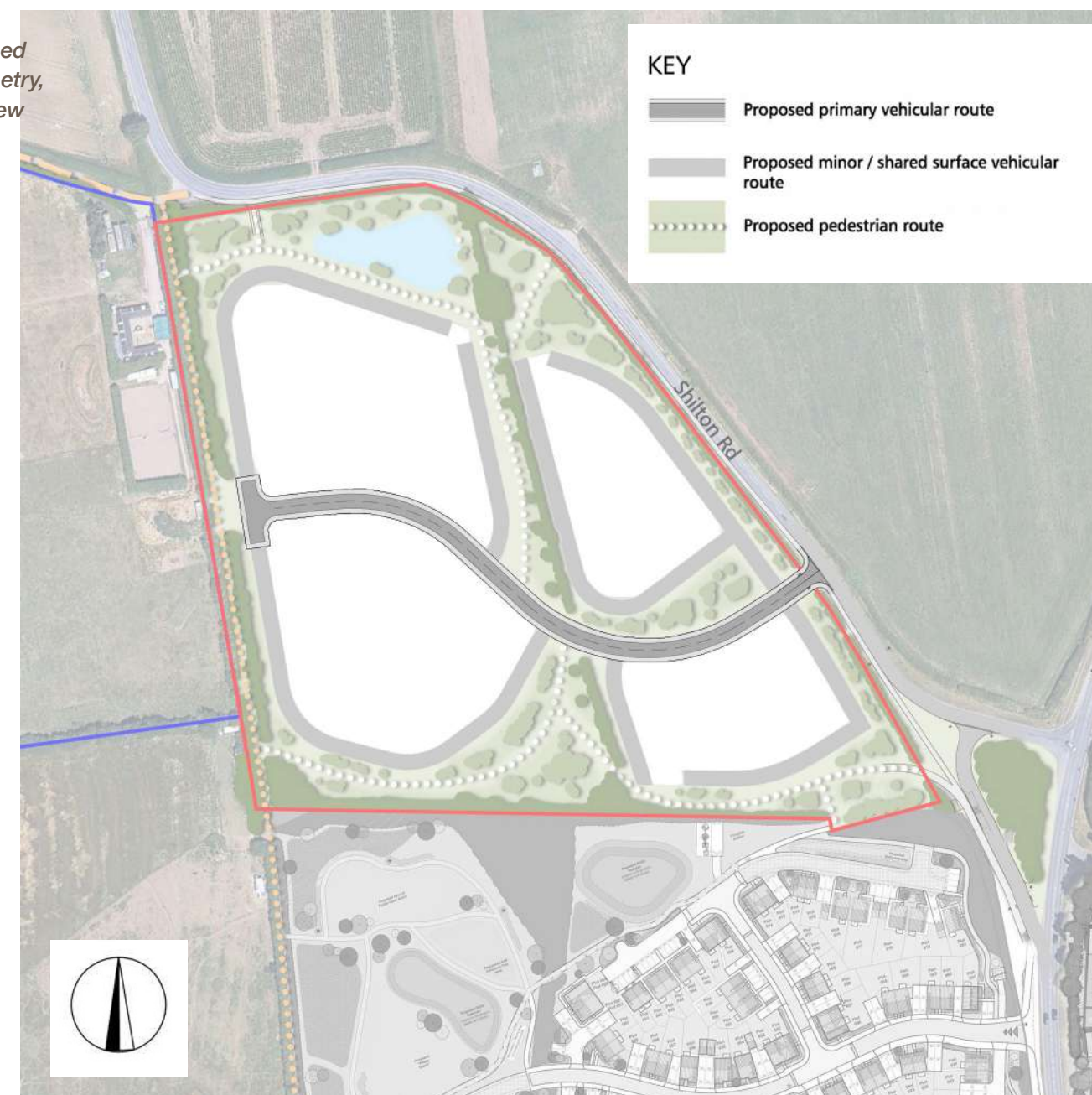


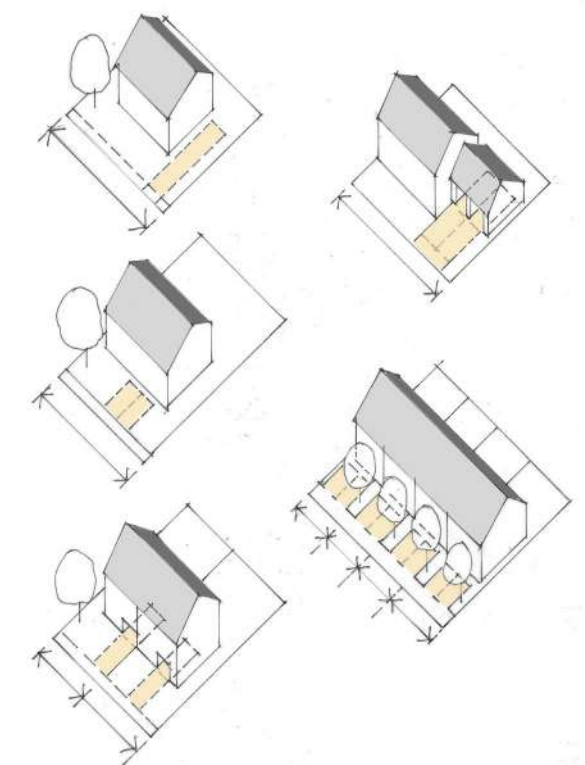
Figure 18: Proposed Road Hierarchy Plan



Car Parking

Car parking provision will be in accordance with Leicestershire Highways Design Guidance.

This will primarily be off street and fully integrated into the design and development layout. The sketches and photographs (below) illustrate acceptable frontage and in plot car parking solutions.



(Above)
The plan shows the proposed layout road hierarchy with a curved street primary access road the feeds secondary minor and 'homezone' streets defining perimeter block development. The linking pedestrian routes through the green open space are also shown, ceating a permeable and well connected housing layout.



LAND WEST OF SHILTON ROAD, EARL SHILTON

CONCLUSION

Delivery

The site is intended to be built in one single phase and would be suitable for a single housebuilder. Build-out rates would be subject to market demand. On the basis of strong delivery across comparable sites in Earl Shilton in recent years, completion of up to 50 dwellings per annum would be expected. The site could therefore be built out in three to four years from commencement, allowing time for ground works and primary infrastructure.

This Design and Access Statement has provided a detailed contextual assessment and design analysis of the proposed scheme. It has demonstrated how the development is informed and establishes specific design and access principles.

Residential development as proposed will make a positive contribution to the local environment in order to meet local housing need including a significant proportion of Affordable Homes. The development will integrate into the existing settlement pattern of Earl Shilton whilst proposing development at an appropriate scale and layout, and associated access and landscaping.

On this basis, the proposal represents sustainable development. It is responsive and represents good design, with matters of detail to be agreed as part of future reserved matters applications. The proposal is in accordance with the requirements of relevant national and local planning (including design) policies. The proposal is a well thought out design which reflects current national design guidance good practice.



Figure 19: Indicative Aerial Overview and Village Context



Giles Stanley Ltd

