
From: William Marshall <william.marshall@dppukltd.com>
Sent: 07 January 2025 16:57
To: Tim Hartley
Cc: Ryan Evans; Mark Lane
Subject: RE: 24/00828/REM - Land south of 295 Main St
Attachments: SUB_PL_001 Materials Layout Rev A.pdf; SUB_PL_001 Planning Layout Rev B.pdf; 24005-BMC-25-XX-DR-C-811 [P01] Visibility Plan.pdf; 24005-BMC-25-XX-DR-C-812 [P01] Refuse Vehicle Tracking.pdf; 24005-BMC-25-XX-DR-C-813 [P01] Large Car Vehicle Tracking.pdf

Hi Tim,

Happy New Year. I hope you're well and had a good festive break.

We have been busy working on making amendments to the layout based on the comments from yourself, urban design and highways. I have summarised our changes on the attached where they address comments received.

1. Highways

1. All dimensions are now shown on the internal roads, footways and visibility splays.
2. Footways are 2m.
3. Drop kerbs provided for the Public Right of Way.
4. Speed table and Plot 7 conflicts now resolved.
5. I understand that bend widening has now been included and internal radii are followed as per the LHDG.
6. Regarding shared private driveway widths, those serving more than 5 dwellings are the required 4.8m whilst those serving five or less are the required 4.25m width.
7. All driveways should now meet the required width accounting for walls/fences, except for the end of the driveway for plot 5 – however the driveway is unimpeded for the first 8m.
8. All trees are at least 1m from the public highway.

2. Urban Design

1. Brick walls added to the requested plots (12, 16, 22, 26, 40, 47 and 50.
2. Road widths should now correlate.
3. Re the footpath, I'm not sure if you're talking of the Public Right of Way or other footpaths. The other footpaths will be hoggin gravel paths whilst the PRow will be tarmac.
4. Hopefully the location of plot 40 is now satisfactory as is the landscaping arrangement between the PRow and plot 47.
5. We haven't been able to add tree planting between the car parking spaces as there just isn't enough room unfortunately. However, we have now moved the parking to be away from the open space and introduced planting at the southern end to help screen the parking.
6. Planting updated accordingly.
7. Regarding garden sizes, we are achieving the back to back and rear garden lengths as set out in the guidance however it is true that some gardens do not meet either the 60sqm or 80sqm as required. I reviewed the committee you highlighted and it is true that garden sizes were mentioned but it's not clear to me how much this contributed to a deferral as there were other issues as well. I think our first question here would be whether this application would be going to planning committee given that it is a reserved matters application – can you confirm?
Secondly, I have counted the garden sizes and 56% are meeting or within 10% of the guidance. This then rises to 64% when looking at within 10sqm. We have explored increasing the sizes but there is no easy way of ensuring that all units have the minimum according to guidance as there is always a knock on effect on either parking, other units, the road layout or soft landscaping. Our argument would be that as there is a significant amount of on-site open space, this makes up for smaller gardens as residents can, for example, go to the open space to kick a ball about or do activities reserved for larger gardens. We'd welcome your thoughts on this.

We're happy to discuss the proposals further in a meeting or phone call if easier.

Thanks,

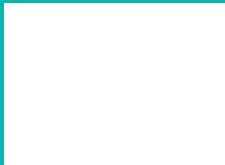
William Marshall - **Principal Planner**

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From: Tim Hartley <Tim.Hartley@hinckley-bosworth.gov.uk>

Sent: 28 November 2024 09:54

To: Harriet Whalley <harriet.whalley@dppukltd.com>

Cc: Ryan Evans <Ryan.Evans@allison-homes.com>; William Marshall <william.marshall@dppukltd.com>; Mark Lane <mark.lane@dppukltd.com>

Subject: 24/00828/REM - Land south of 295 Main St

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Hi Harriet

I received the request below from the Highway Authority yesterday

I received comments from our Road Adoptions team and they are asking whether it would be possible for the Applicant to indicate the amended road layout on the same drawing as the previous Allison homes

drawing titled 'Planning Layout' drawing no. SUB/PL/001 Revision A? This will then mean that they can assess the new road layout with the location of the proposed dwellings and condition the same drawing.

Can this be provided please?

Many thanks

Tim

Tim Hartley
Team Leader Development Management

Tel: 01455 255656

Email: tim.hartley@hinckley-bosworth.gov.uk

Web: www.hinckley-bosworth.gov.uk

Hinckley & Bosworth
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Materials

Render

Bricks

Tiles



Webber Ivory Render



Hardwicke Welbeck



Russell Bute 3 Red



Mellor Red Multi



Russell Bute 3 Grey

LEGEND	
	Site Boundary
	1.8m Closeboard Fence
	1.8m Brick Wall (to match adjacent house)
	1.2m Post & Rail Fence
	0.45m Knee Rail Fence (4inch rails)
	1.2M Instant Hedge
	Cheshire Red Multi with Red feature Brick
	Mellor Red Multi with Blue feature Brick
	Grey Roof Tile
	Red Roof Tile
	Indicative GRP Chimney plots
	Render Ivory
	EV Point

Boundary Treatments



1.2m Instant Hedge 1.2m Post and Rail

Elevation Details

- Fascias and Soffits- Black UPVC
- Rainwater goods- Black UPVC
- Front Window Design - Cottage as per planning housetype pack- White UPVC
- Front Door Design - Cottage as per planning housetype pack
- Front Door Colours -Black (garage doors Black)

A

18.12.24

Layout amended to reflect planning comments.


AB

REVISION

DATE

DESCRIPTION

DRAWN



ALLISON HOMES

ALLISON HOMES
LARKFLEET HOUSE
SOUTHFIELDS BUSINESS PARK
BOURNE
LINCOLNSHIRE
PE10 0FF
TEL: 01778 391550

PROJECT

Stanton Under Bardon

DRAWING TITLE

Materials Layout

SCALE

DATE

DRAWN

APPROVED

1:500@ A2

20.08.24

AB

-

DRAWING NO.

REVISION

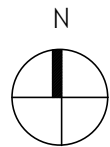
SUB/ML/001

A

Open Market			
House Type	Bed	Storey	No.
Danbury	2	2	6
Gosford	3	2	5
Corby	3	2	4
Lynford	3	2	7
Ashford	3	2	5
Oxford	3	2	6
Eltham	4	2	3
Oakham	4	2	3
Trentham	4	2	3
Northam	4	2	2
			44

Affordable			
House Type	Bed	Storey	No.
Loxley	2	2	4
Morley	3	2	2
			6
			50

LEGEND	
	Site Boundary
	Open Space
	Proposed Tree
	Bin Collection Point
	Plot Number
	Easement
	SUDS
	Affordable Units
	First Homes
	Public Right of Way



B	05.12.24	Layout to reflect planning comments. Affordable allocations also updated to reflect housing officers comments.	RDE
A	20.08.24	Layout amended to reflect preapp. advice	AB
REVISION	DATE	DESCRIPTION	DRAWN



**ALLISON
HOMES**

ALLISON HOMES
THE LOCKINGTON SUITE
STATION ROAD
CASTLE DONINGTON
DERBYSHIRE
DE74 2NJ
TEL: 01778 391550

PROJECT

Stanton Under Bardon

DRAWING TITLE

PLANNING LAYOUT

SCALE	DATE	DRAWN	APPROVED
1:500 @ A2	15.08.24	RDE	-


DRAWING NO.	REVISION
SUB/PL/001	B



GENERAL NOTES





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KEY:

- 
- Development Boundary
 - 17m Forward Visibility
 - 25m Junction Visibility
 - 17m Shared Drive Junction Visibility
 - 2.0m x 2.0m Pedestrian Visibility

Health and safety symbols refer to reference numbers indicated on Designers Risk Assessment number: 24005-BMC-24-XX-HS-C-0001

Health & Safety Information Key

- | | |
|---|--|
|  | Used to provide design specific safety information that may not be obvious to a competent contractor but may be useful |
|  | Used to restrict/prohibit a possible action, e.g. stop construction traffic from entering an area |
|  | Used to warn of significant design hazards, adding recommendations |
|  | Used to encourage a positive action, e.g. use of robust protection for inspection chambers |

P01	06.01.25	Issued for approval.	BRM	-
Rev	Date	Amendments	By	Chk



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Freedom on the Hill
DE73 8LF
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mcengineering.co.uk

Client

ALLISON HOMES

Project

MAIN STREET
STANTON UNDER BARDON

Drawing Title

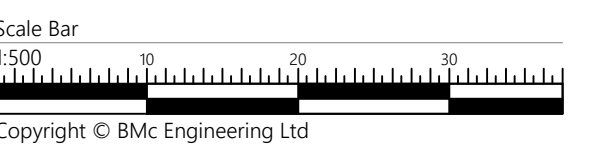
VISIBILITY PLAN

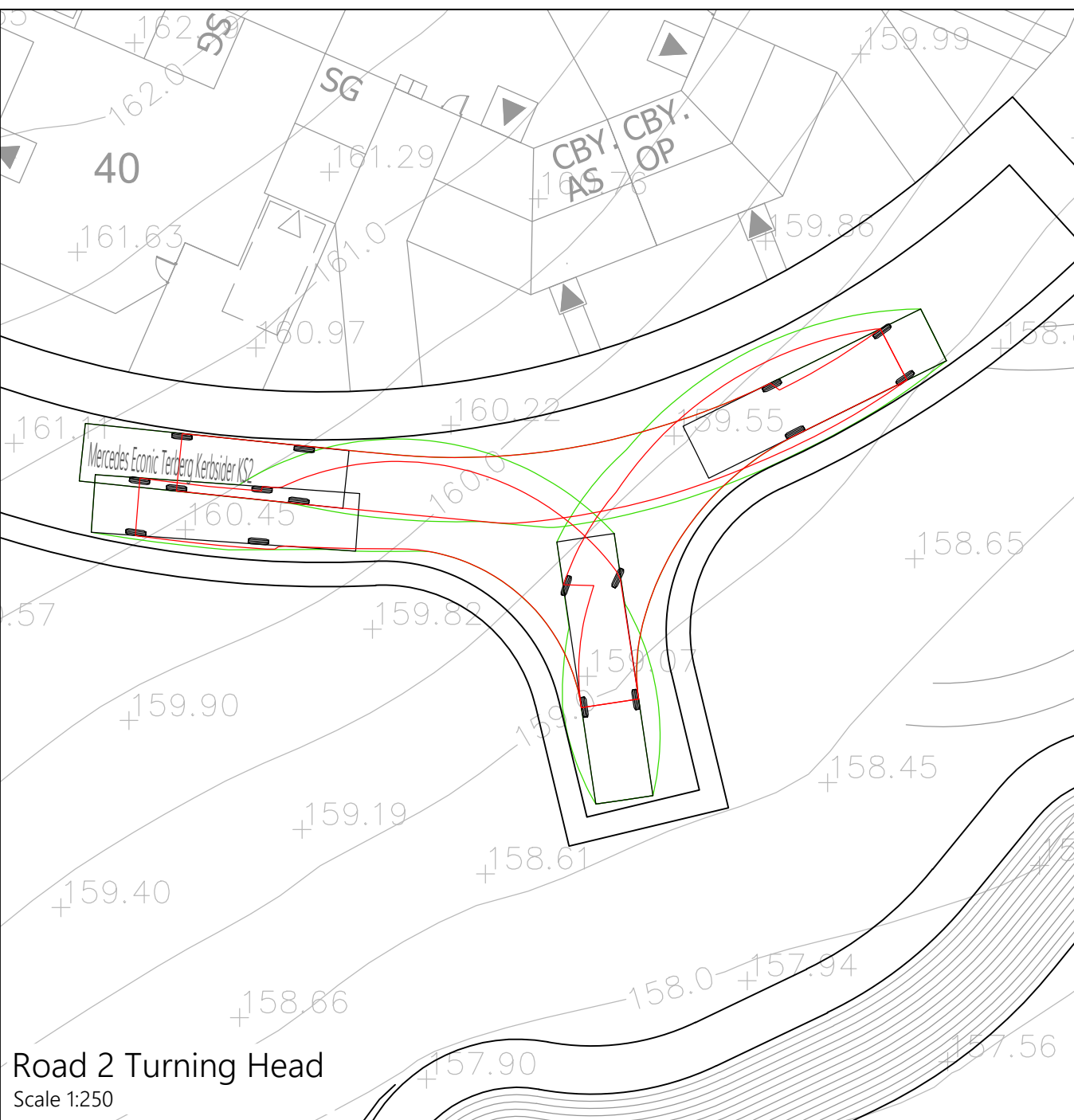
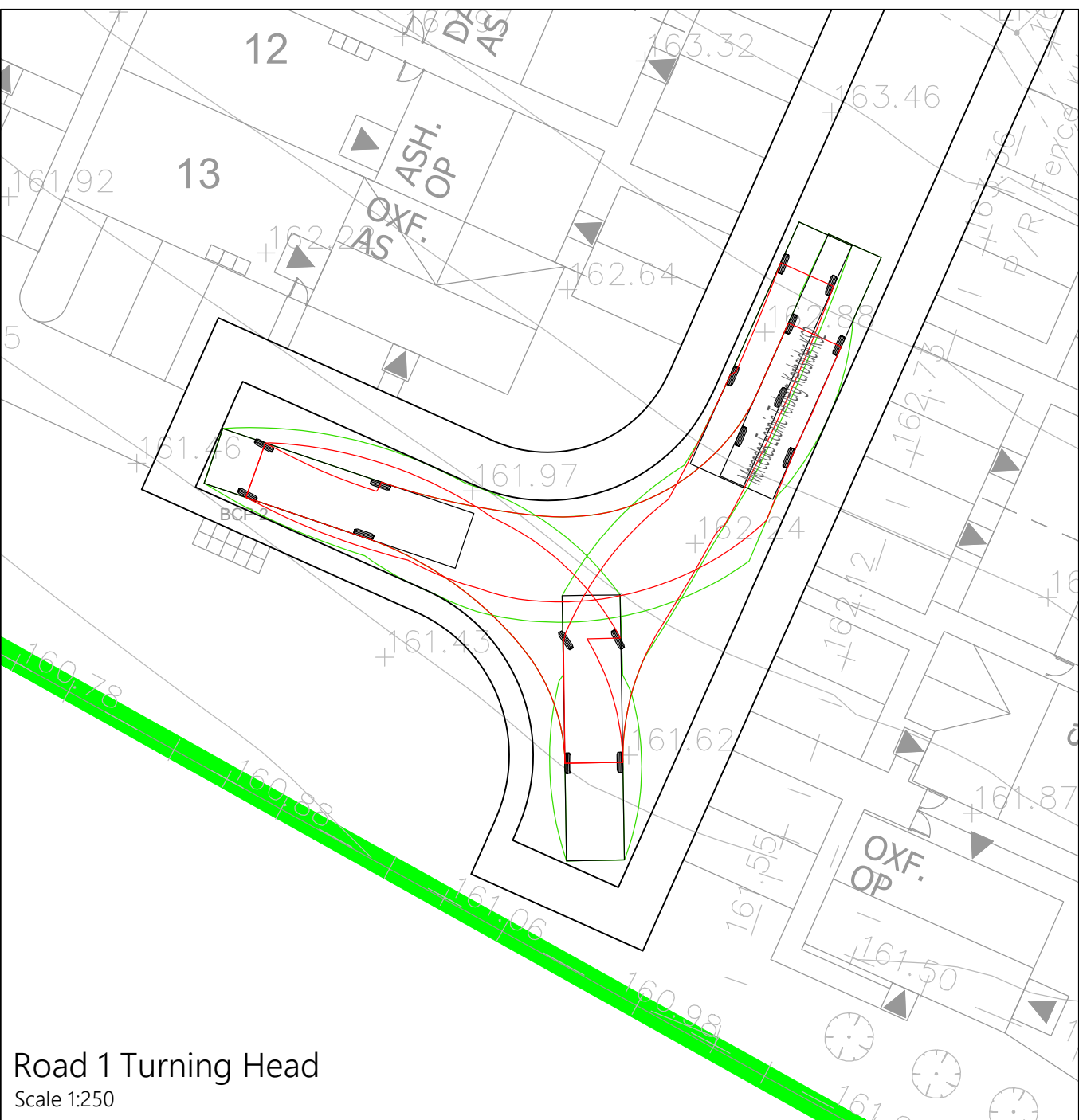
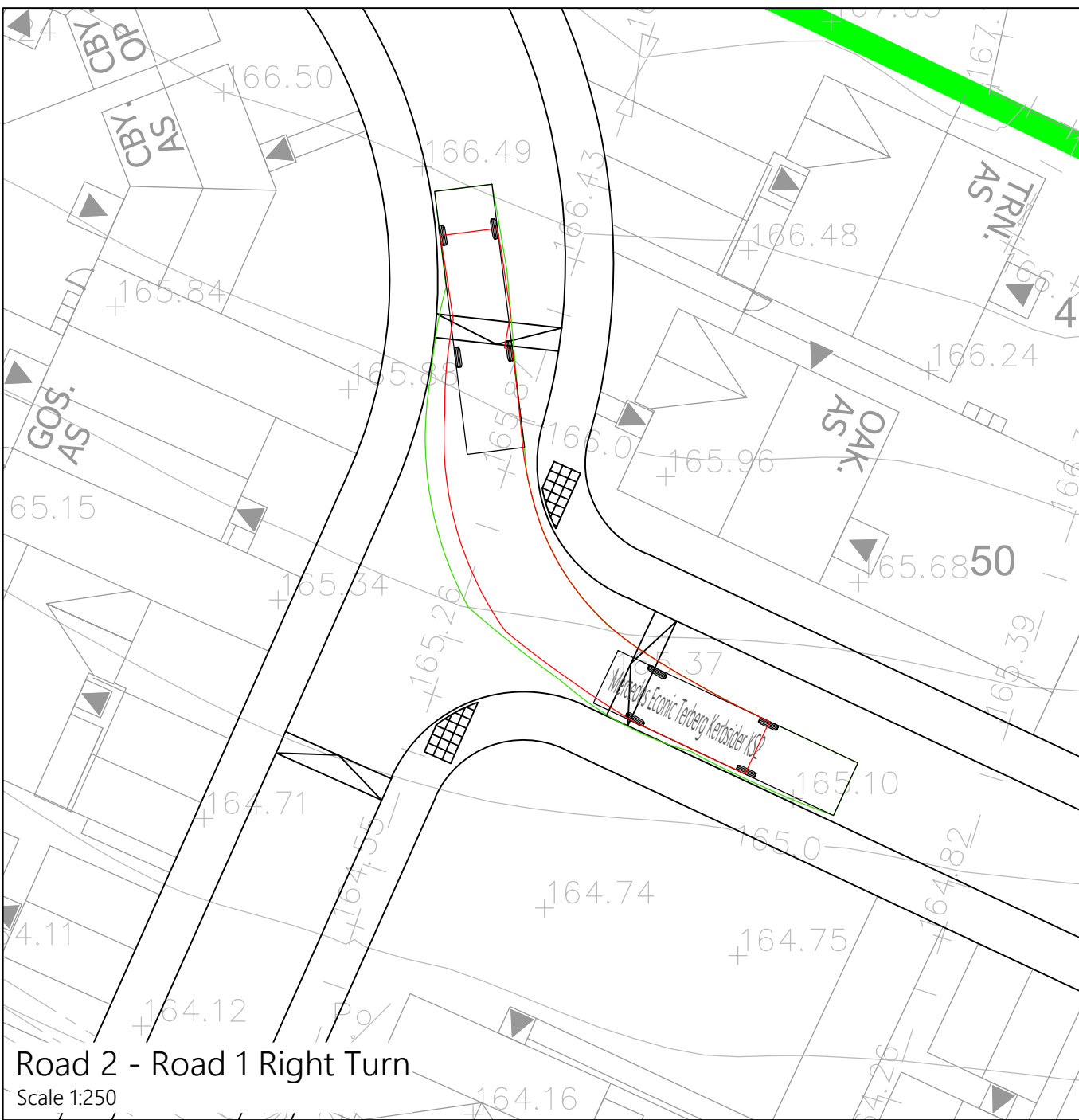
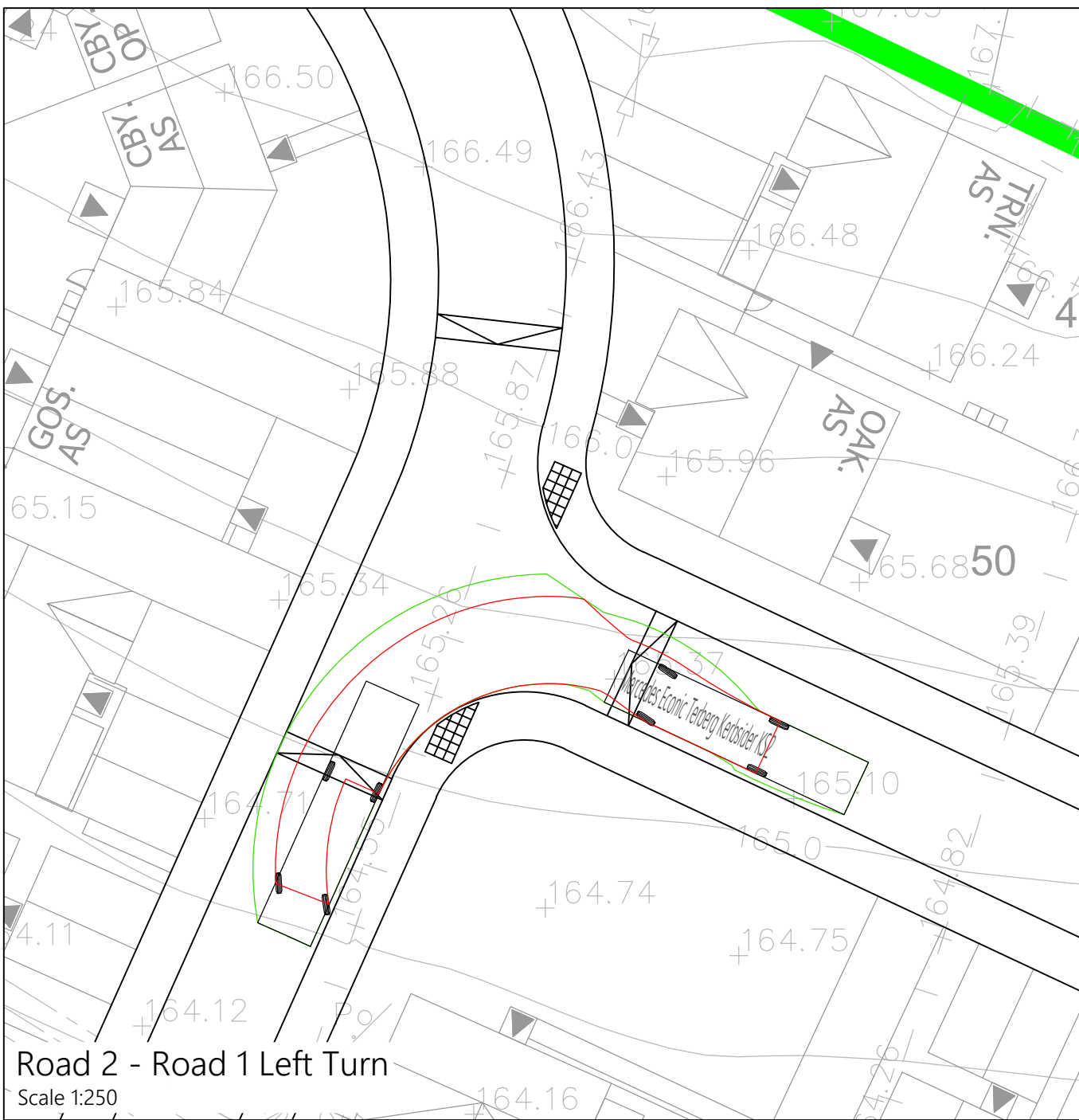
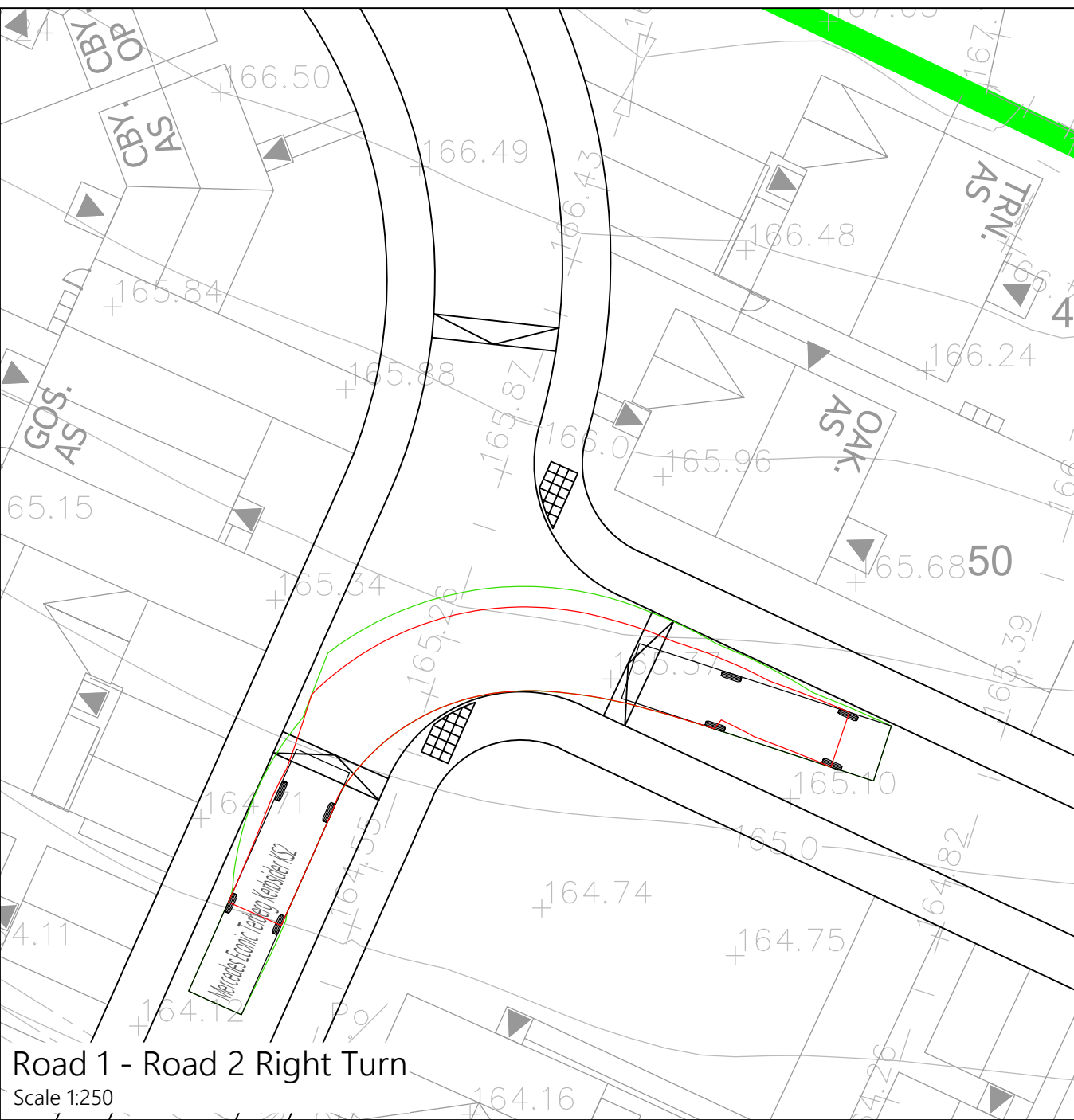
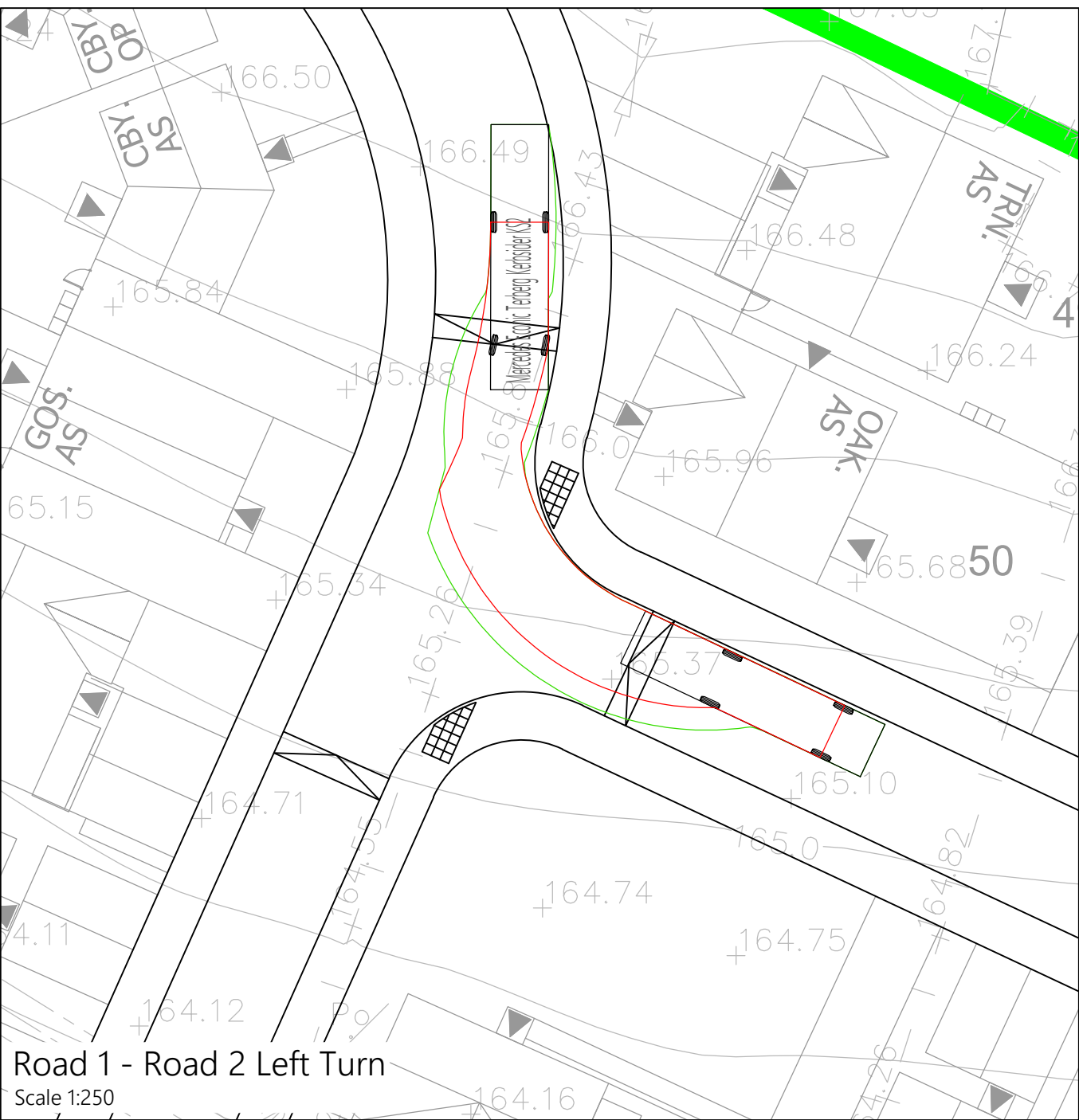
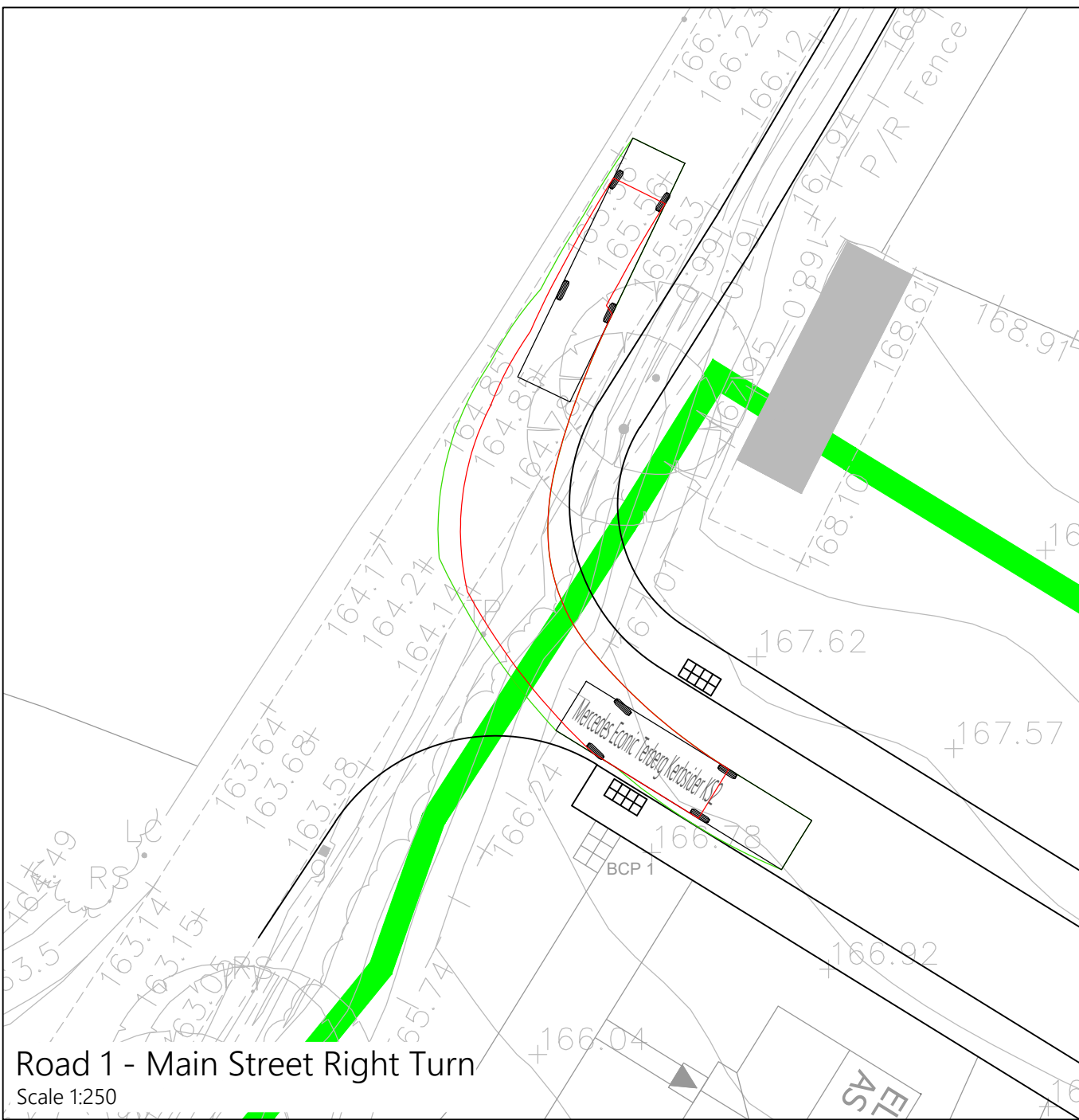
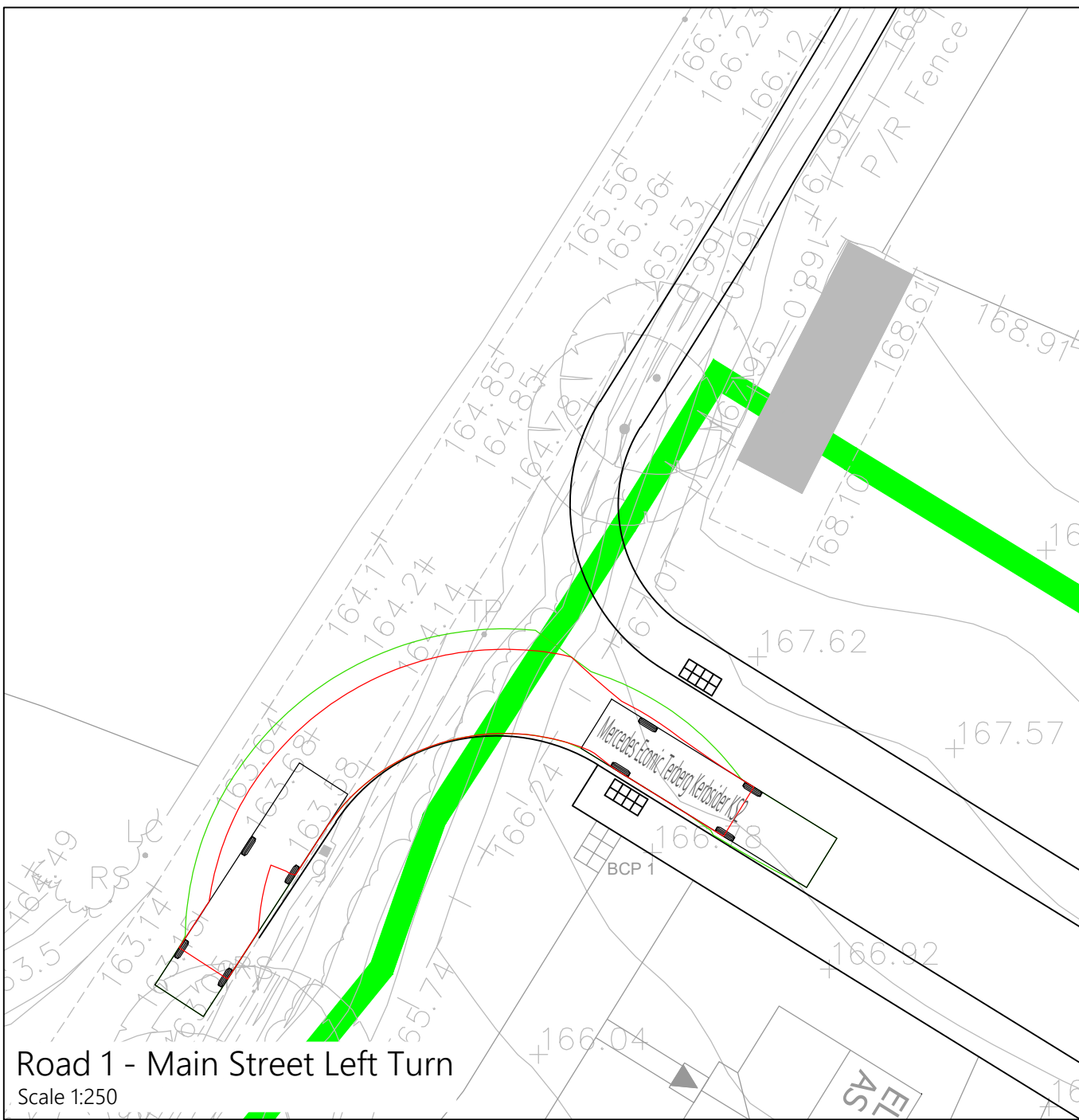
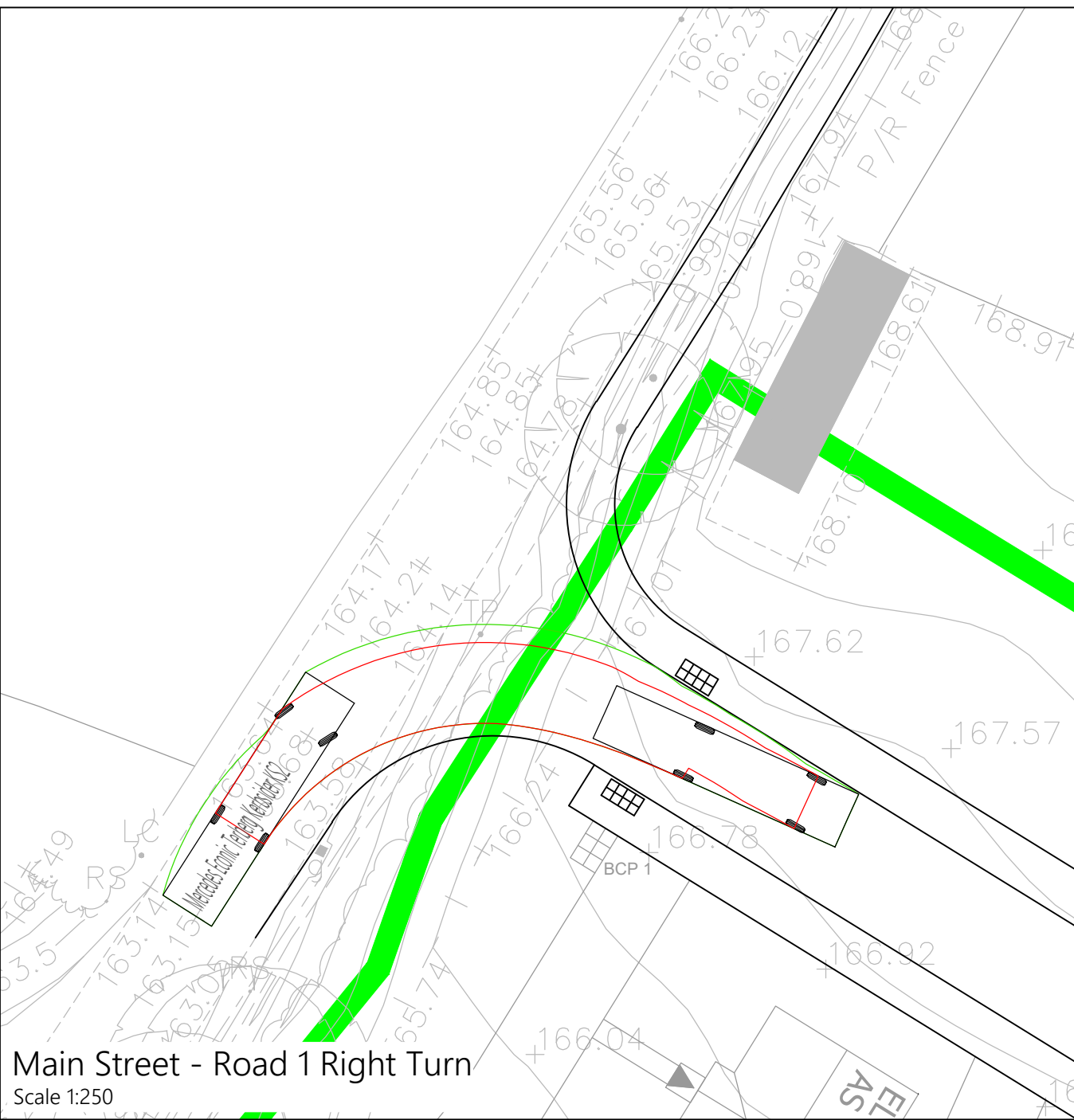
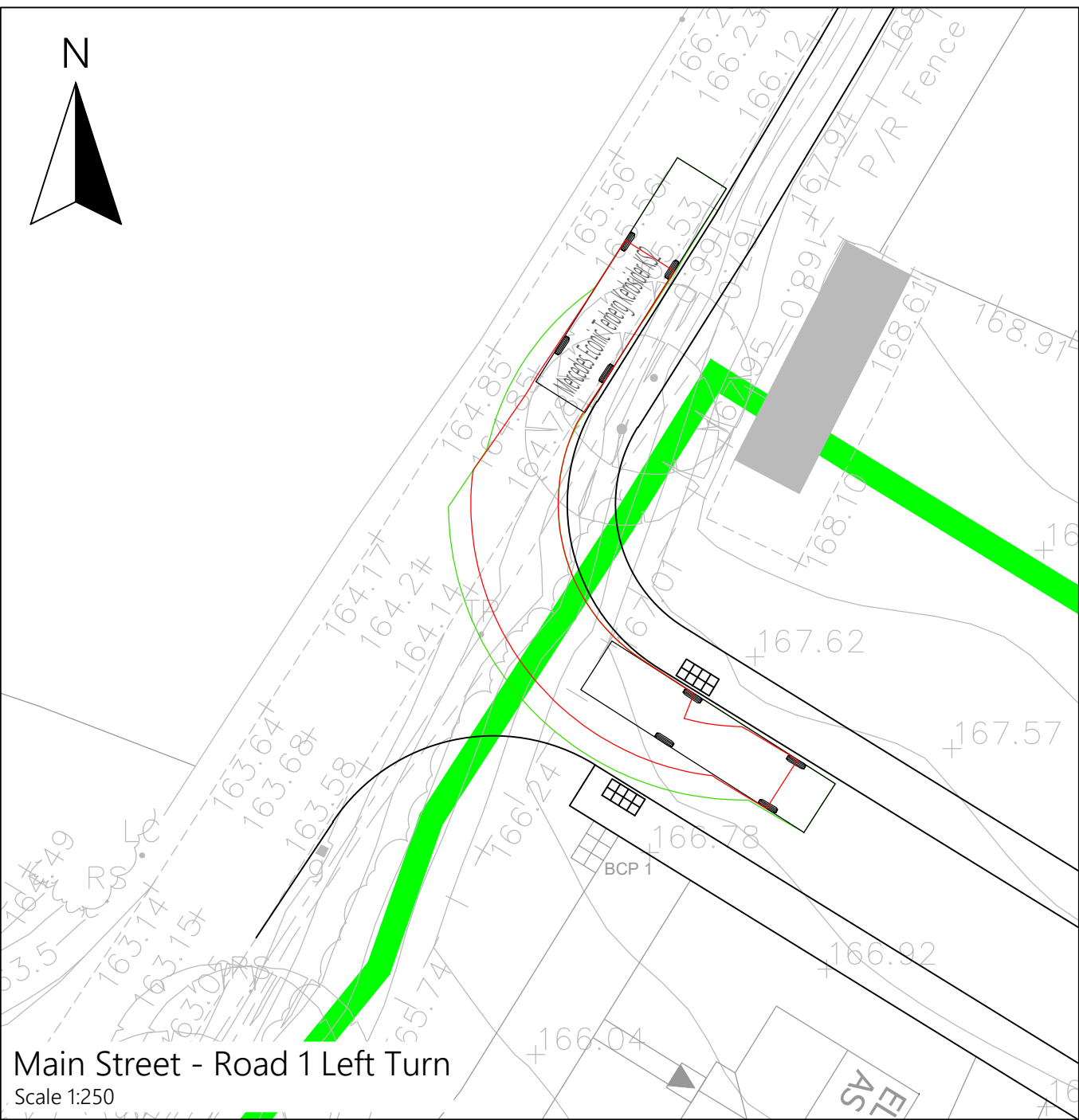
Status

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Scale	Drawn	Checked	Date
1:500 @ A1	BRM	-	06.02.25

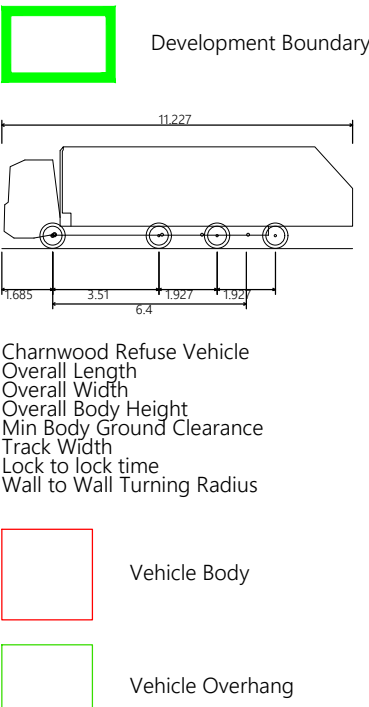
Drawing Number	Revision
24005-BMC-25-XX-DR-C-811	P01





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KEY:



Charmwood Refuse Vehicle
Overall Length 11.227m
Overall Width 2.500m
Overall Body Height 3.365m
Min Body Ground Clearance 0.345m
Track Width 2.400m
Lock to lock time 4.00s
Wall to Wall Turning Radius 11.200m

Health and safety symbols refer to reference numbers indicated on Designers Risk Assessment number: 24005-BMC-24-XX-HS-C-0001

Health & Safety Information Key

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- Used to restrict/prevent a possible action, e.g. stop construction traffic from entering an area
- Used to warn of significant design hazards, adding recommendations
- Used to encourage a positive action, e.g. use of robust protection for inspection chambers

P01	06.01.25	Issued for approval.	BRM	-
Rev	Date	Amendments	By	Chk

BMc
engineering

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bmceengineering.co.uk

Client

ALLISON HOMES

Project

MAIN STREET
STANTON UNDER BARDON

Drawing Title

REFUSE VEHICLE TRACKING

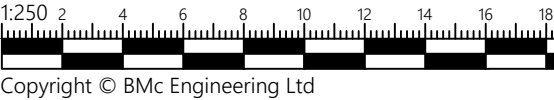
Status

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Scale	Drawn	Checked	Date
1:250 @ A1	BRM	-	06.01.25

Drawing Number	Revision
24005-BMC-25-XX-DR-C-812	P01

Scale Bar



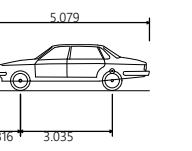
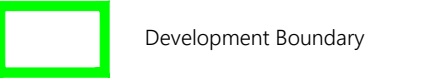
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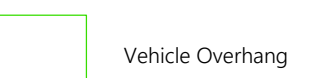
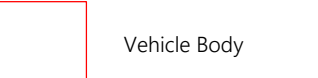
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KEY:







Large Car (2006)	
Overall Length	5.079m
Overall Width	1.872m
Overall Body Height	1.525m
Min Body Ground Clearance	0.310m
Max Track Width	1.831m
Lock to lock time	4.00s
Kerb to Kerb Turning Radius	5.900m



Health and safety symbols refer to reference numbers indicated on Designers Risk Assessment number: 24005-BMC-24-XX-HS-C-0001

Health & Safety Information Key

- | | |
|---|--|
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P01	06.01.25	Issued for approval.	BRM	-
Rev	Date	Amendments	By	Chk



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Client

ALLISON HOMES

Project

MAIN STREET
STANTON UNDER BARDON

Drawing Title

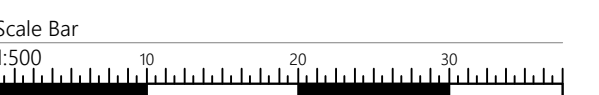
LARGE CAR VEHICLE TRACKING

Status

FOR APPROVAL

Scale	Drawn	Checked	Date
1:500 @ A1	BRM	-	06.01.25

Drawing Number	Revision
24005-BMC-25-XX-DR-C-813	P01



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