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**From:** William Marshall <william\_marshall@dppukltd.com>  
**Sent:** 07 January 2025 16:57  
**To:** Tim Hartley  
**Cc:** Ryan Evans; Mark Lane  
**Subject:** RE: 24/00828/REM - Land south of 295 Main St  
**Attachments:** SUB\_PL\_001 Materials Layout Rev A.pdf; SUB\_PL\_001 Planning Layout Rev B.pdf; 24005-BMC-25-XX-DR-C-811 [P01] Visibility Plan.pdf; 24005-BMC-25-XX-DR-C-812 [P01] Refuse Vehicle Tracking.pdf; 24005-BMC-25-XX-DR-C-813 [P01] Large Car Vehicle Tracking.pdf

Hi Tim,

Happy New Year. I hope you're well and had a good festive break.

We have been busy working on making amendments to the layout based on the comments from yourself, urban design and highways. I have summarised our changes on the attached where they address comments received.

1. Highways

1. All dimensions are now shown on the internal roads, footways and visibility splays.
2. Footways are 2m.
3. Drop kerbs provided for the Public Right of Way.
4. Speed table and Plot 7 conflicts now resolved.
5. I understand that bend widening has now been included and internal radii are followed as per the LHDG.
6. Regarding shared private driveway widths, those serving more than 5 dwellings are the required 4.8m whilst those serving five or less are the required 4.25m width.
7. All driveways should now meet the required width accounting for walls/fences, except for the end of the driveway for plot 5 – however the driveway is unimpeded for the first 8m.
8. All trees are at least 1m from the public highway.

2. Urban Design

1. Brick walls added to the requested plots (12, 16, 22, 26, 40, 47 and 50).
2. Road widths should now correlate.
3. Re the footpath, I'm not sure if you're talking of the Public Right of Way or other footpaths. The other footpaths will be hoggin gravel paths whilst the PRoW will be tarmac.
4. Hopefully the location of plot 40 is now satisfactory as is the landscaping arrangement between the PRoW and plot 47.
5. We haven't been able to add tree planting between the car parking spaces as there just isn't enough room unfortunately. However, we have now moved the parking to be away from the open space and introduced planting at the southern end to help screen the parking.
6. Planting updated accordingly.
7. Regarding garden sizes, we are achieving the back to back and rear garden lengths as set out in the guidance however it is true that some gardens do not meet either the 60sqm or 80sqm as required. I reviewed the committee you highlighted and it is true that garden sizes were mentioned but it's not clear to me how much this contributed to a deferral as there were other issues as well. I think our first question here would be whether this application would be going to planning committee given that it is a reserved matters application – can you confirm?

Secondly, I have counted the garden sizes and 56% are meeting or within 10% of the guidance. This then rises to 64% when looking at within 10sqm. We have explored increasing the sizes but there is no easy way of ensuring that all units have the minimum according to guidance as there is always a knock on effect on either parking, other units, the road layout or soft landscaping. Our argument would be that as there is a significant amount of on-site open space, this makes up for smaller gardens as residents can, for example, go to the open space to kick a ball about or do activities reserved for larger gardens. We'd welcome your thoughts on this.

We're happy to discuss the proposals further in a meeting or phone call if easier.

Thanks,

**William Marshall - Principal Planner**

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**From:** Tim Hartley <Tim.Hartley@hinckley-bosworth.gov.uk>

**Sent:** 28 November 2024 09:54

**To:** Harriet Whalley <harriet.whalley@dppukltd.com>

**Cc:** Ryan Evans <Ryan.Evans@allison-homes.com>; William Marshall <william.marshall@dppukltd.com>; Mark Lane <mark.lane@dppukltd.com>

**Subject:** 24/00828/REM - Land south of 295 Main St

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Hi Harriet

I received the request below from the Highway Authority yesterday

I received comments from our Road Adoptions team and they are asking whether it would be possible for the Applicant to indicate the amended road layout on the same drawing as the previous Allison homes

drawing titled 'Planning Layout' drawing no. SUB/PL/001 Revision A? This will then mean that they can assess the new road layout with the location of the proposed dwellings and condition the same drawing.

Can this be provided please?

Many thanks

Tim

**Tim Hartley**  
**Team Leader Development Management**

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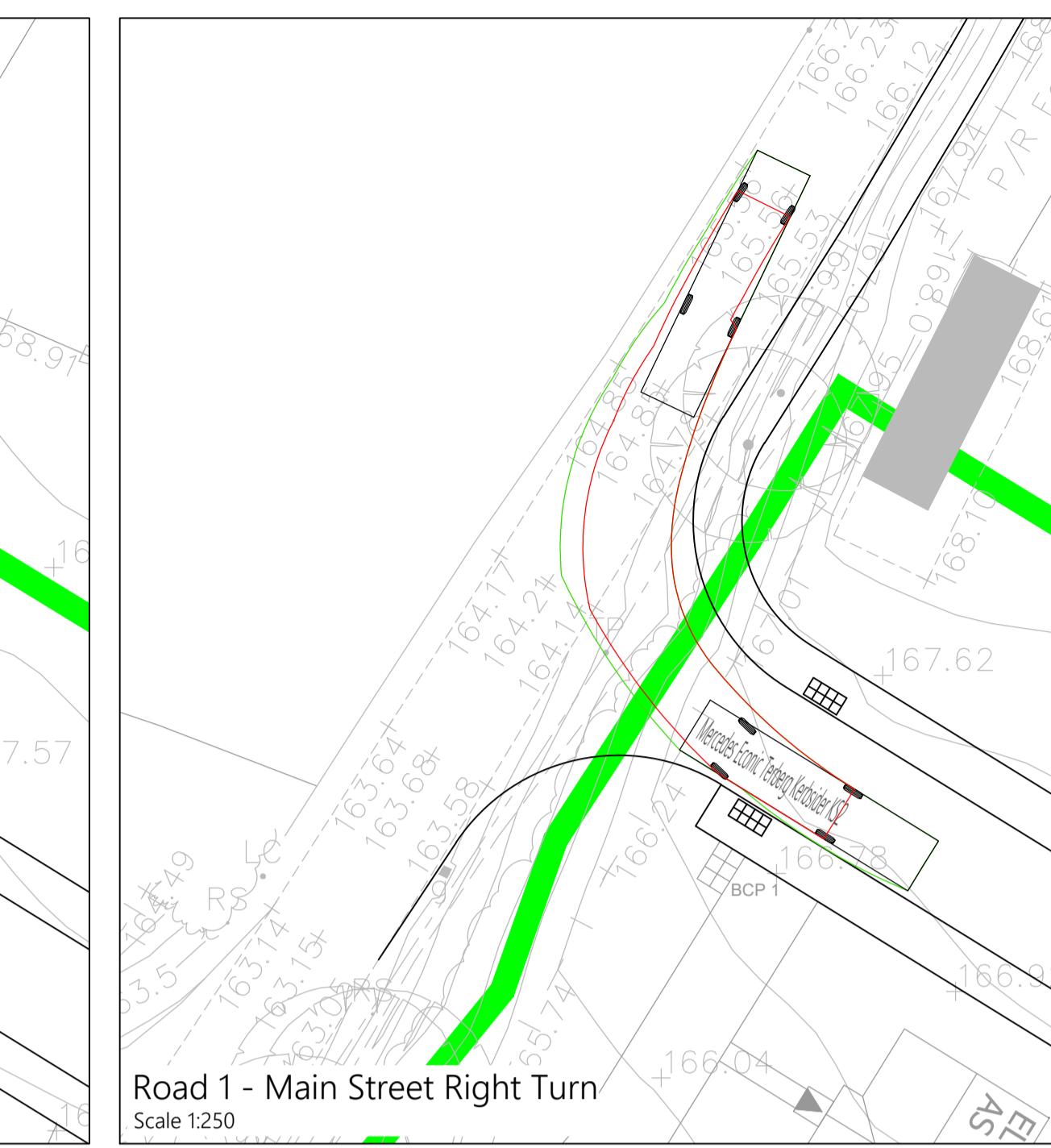
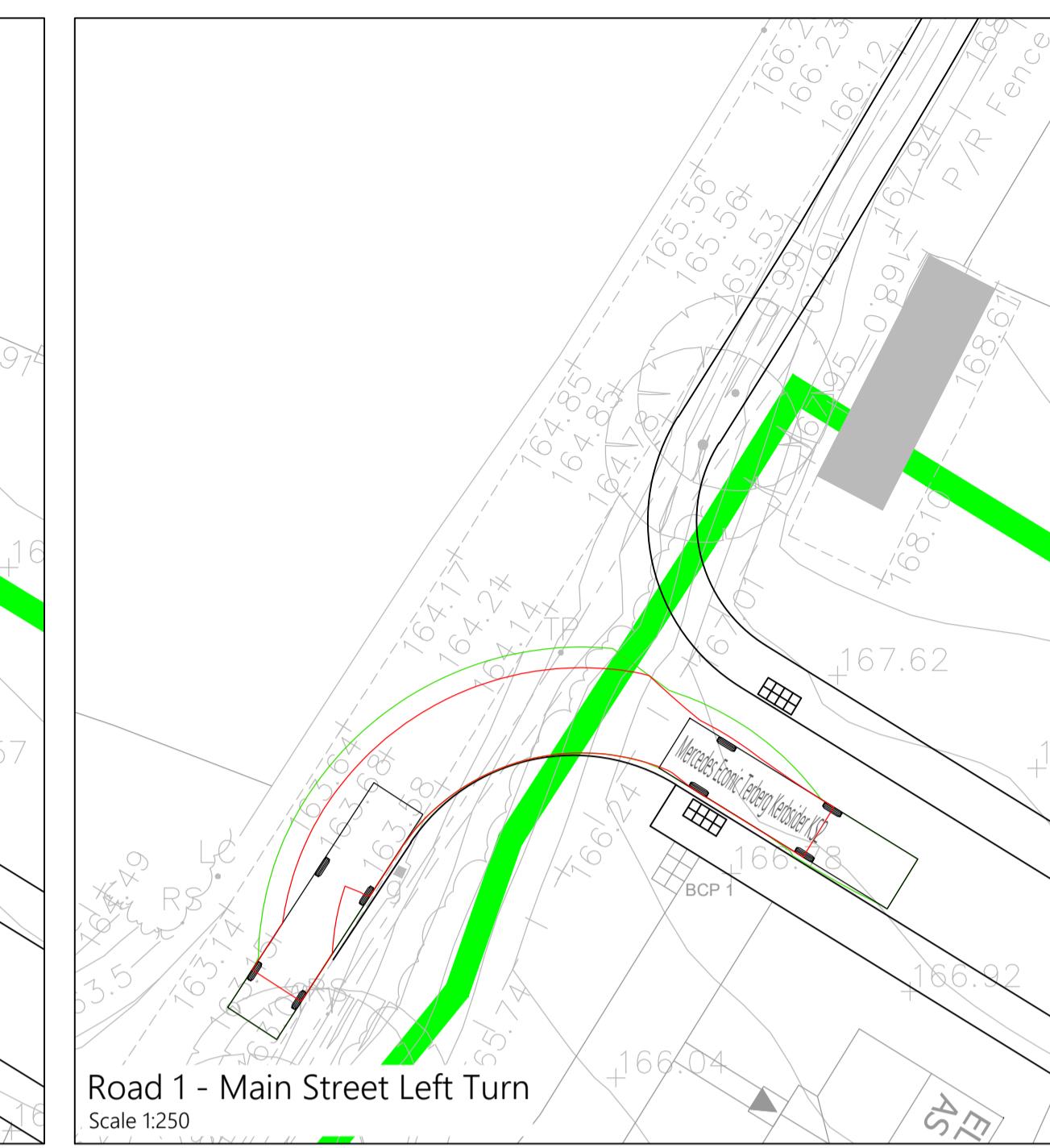
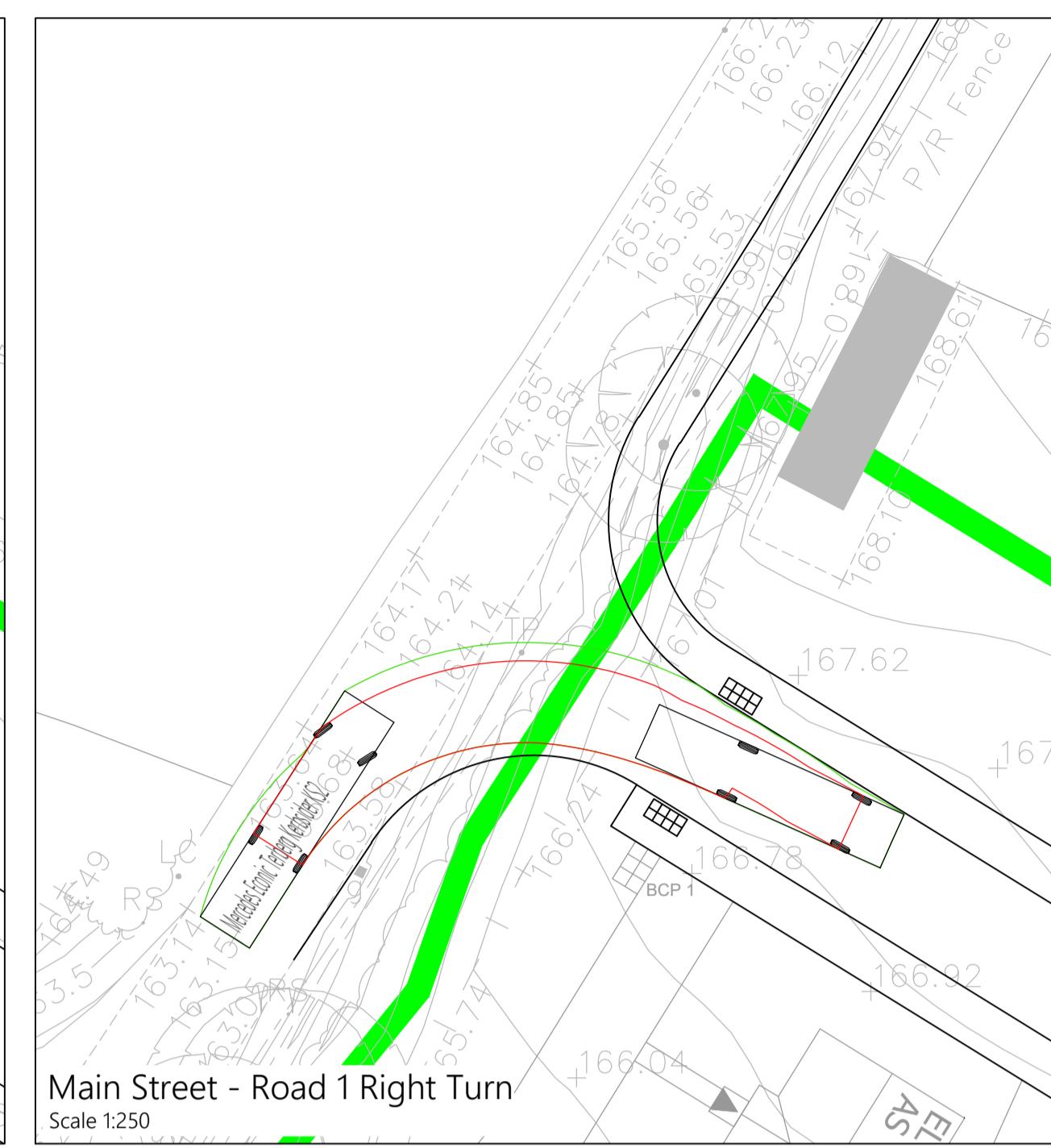
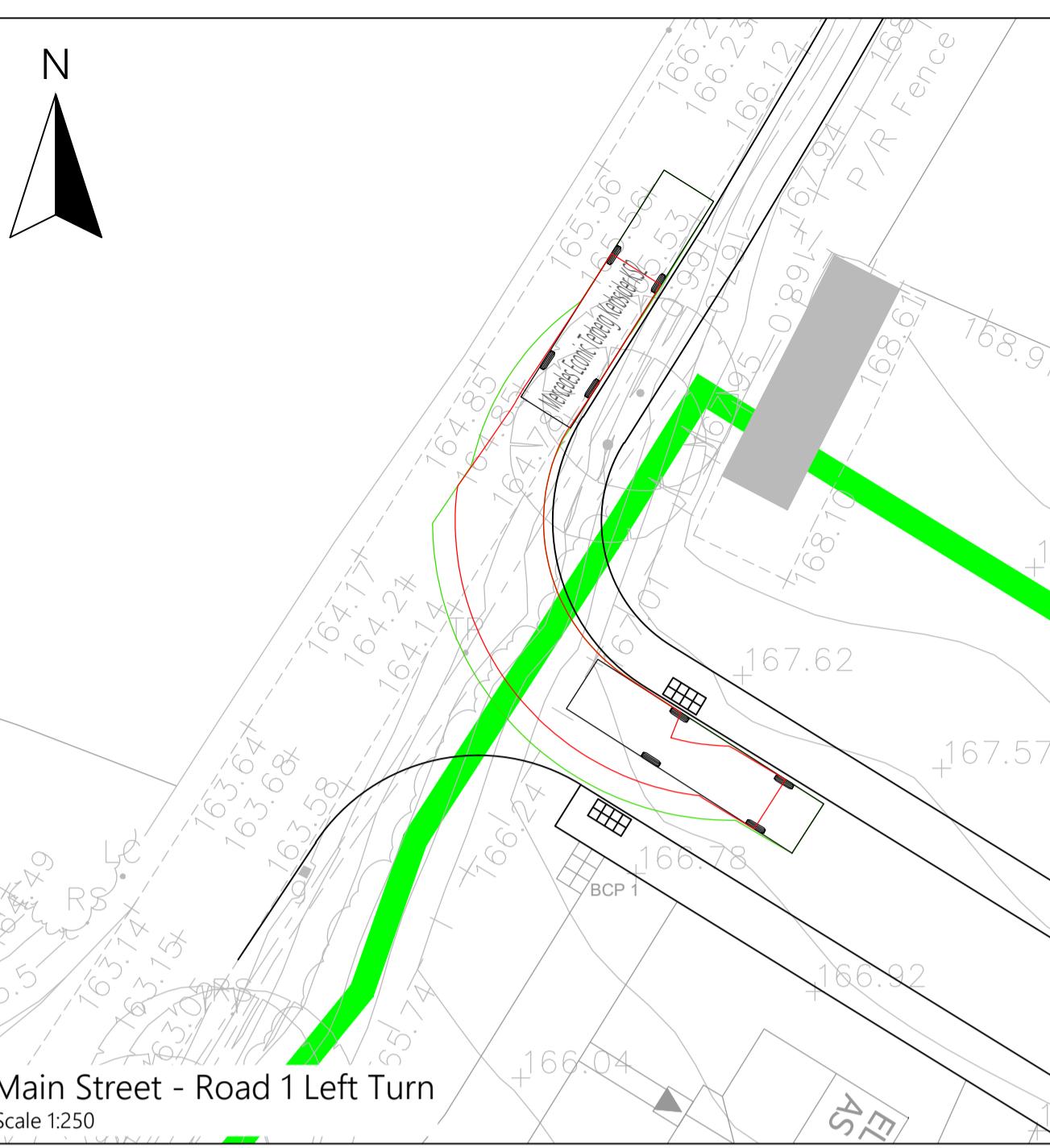
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- This drawing is to be read in conjunction with all other relevant drawings and details.
- Should there be any conflict between the details indicated on this drawing and other drawings the Engineer should be informed PRIOR to construction on site.
- Until technical approval has been obtained from the relevant Authority, it should be understood that all drawings issued are Preliminary and NOT for construction. Should the Contractor commence site works prior to such approval being provided it is entirely at their own risk.
- Sketch proposals are for illustrative purposes only and as such are subject to detailed site investigation including ground conditions, contaminants, drainage, design and planning / density negotiations.
- All dimensions are in metres unless otherwise stated.
- The Designer's Risk Assessment(s) for this project must be reviewed PRIOR to the commencement of any works.

**KEY:**

Development Boundary

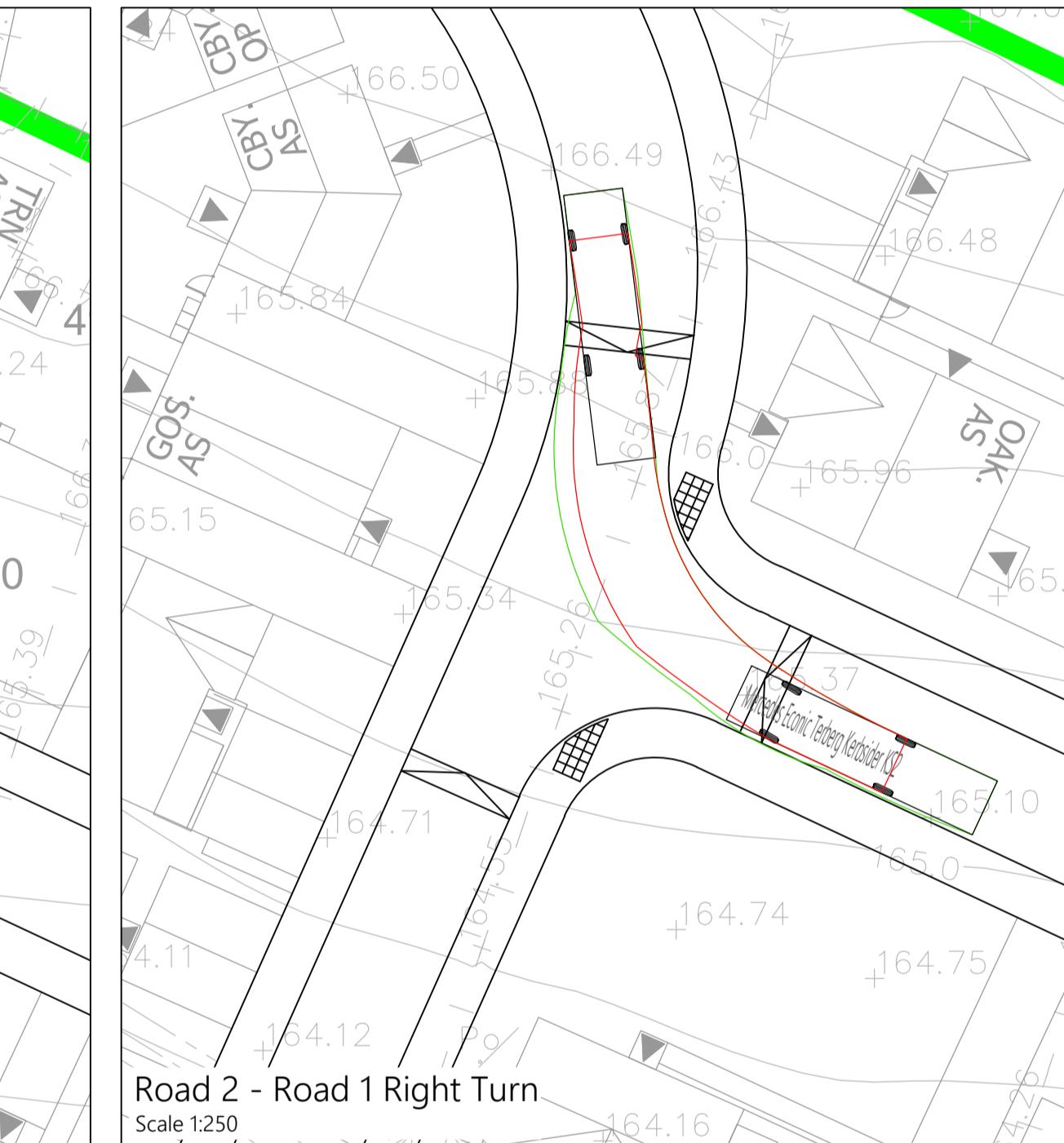
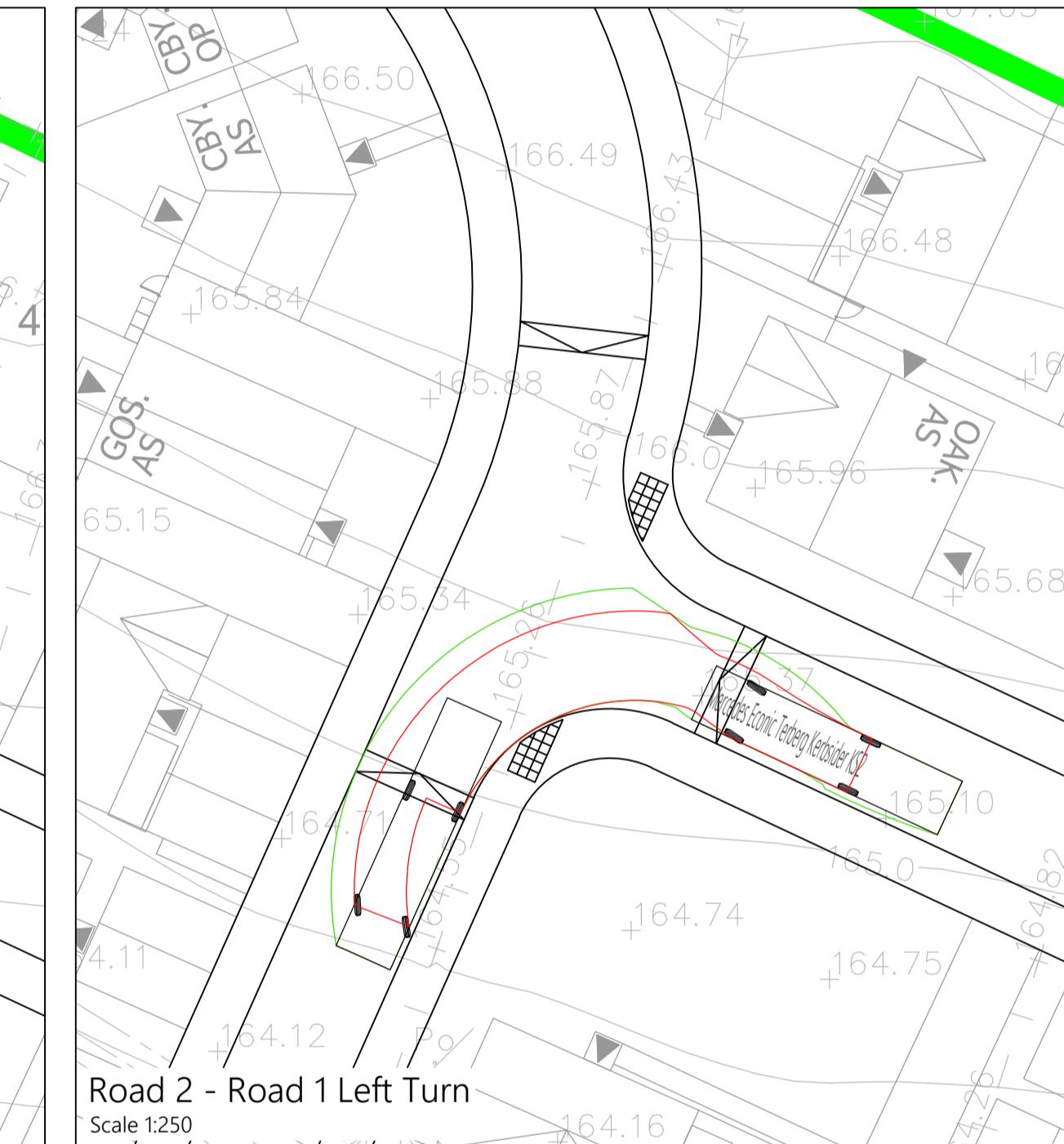
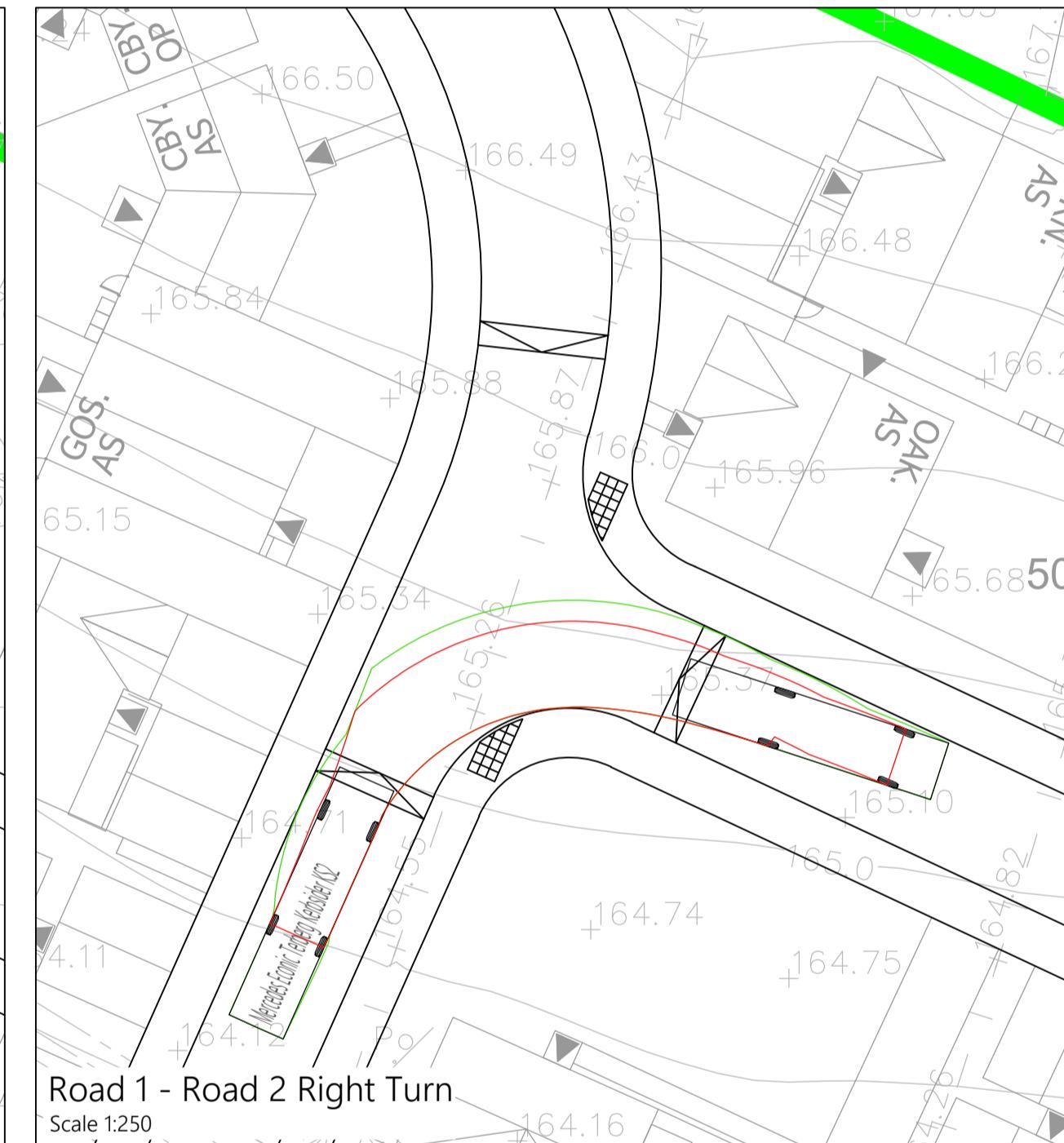
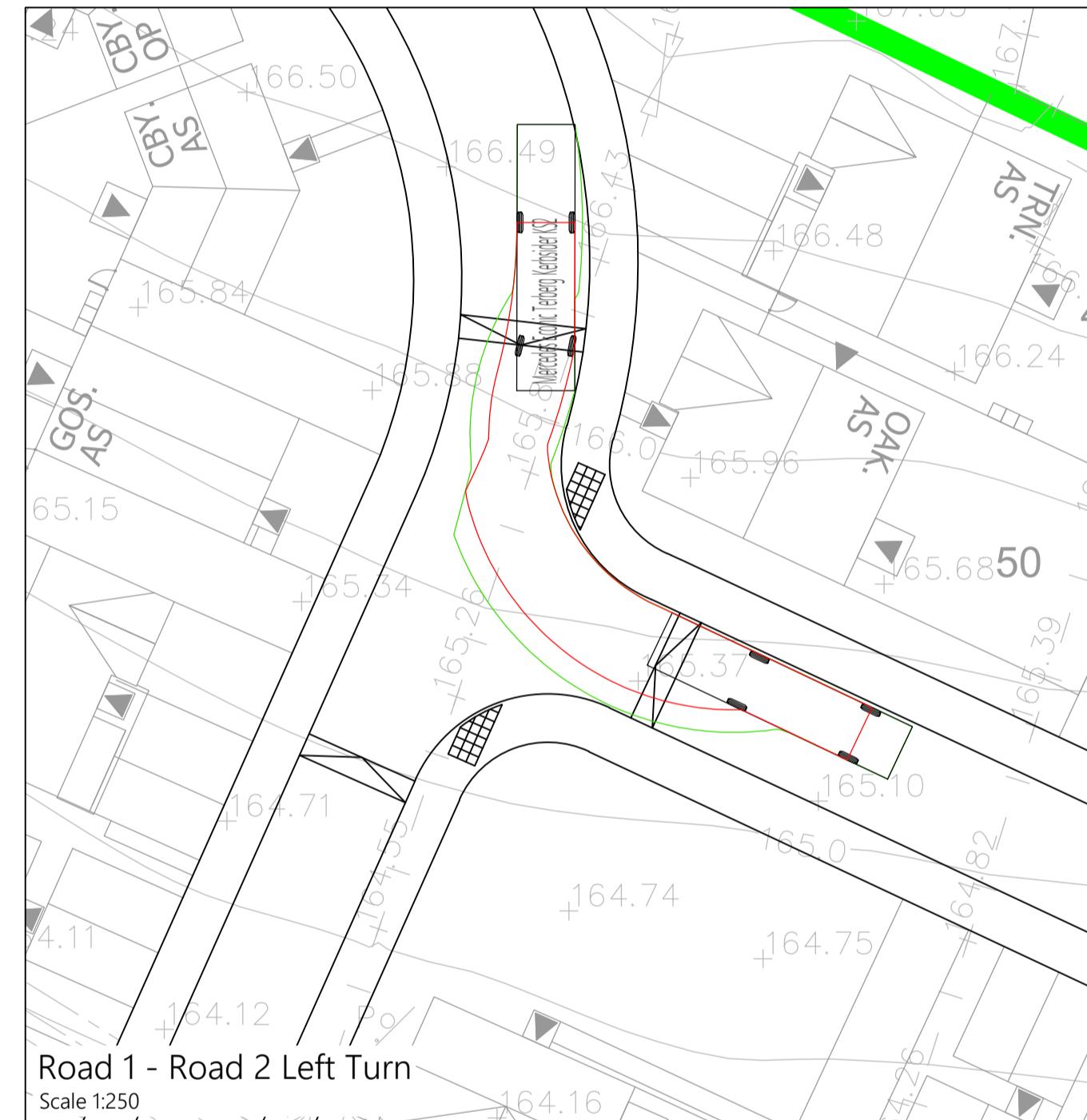
Vehicle Body

Vehicle Overhang

11.227m  
2.500m  
2.365m  
0.445m  
2.400m  
4.00s  
11.200m

Charmwood Refuse Vehicle  
Overall Width  
Overall Body Height  
Min. Site Ground Clearance  
Track Width  
Lock to lock time  
Wall to Wall Turning Radius

11.227m  
2.500m  
2.365m  
0.445m  
2.400m  
4.00s  
11.200m



Health and safety symbols refer to reference numbers indicated on Designers Risk Assessment number: 24005-BMC-24-XX-HS-C-0001

**Health & Safety Information Key**

Used to provide design specific safety information that may not be obvious to a competent contractor but may be useful

Used to restrict/prevent a possible action, e.g. stop construction traffic from entering an area

Used to warn of significant design hazards, adding recommendations

Used to encourage a positive action, e.g. use of robust protection for inspection chambers

P01 06.01.25 Issued for approval. BRM -  
Rev Date Amendments By Chk

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Client

ALLISON HOMES

Project

MAIN STREET  
STANTON UNDER BARDON

Drawing Title

REFUSE VEHICLE TRACKING

Status

FOR APPROVAL

Scale Drawn Checked Date  
1:250 @ A1 BRM - 06.01.25

Drawing Number Revision  
24005-BMC-25-XX-DR-C-812 P01

Scale Bar

1:250 2 4 6 8 10 12 14 16 18

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